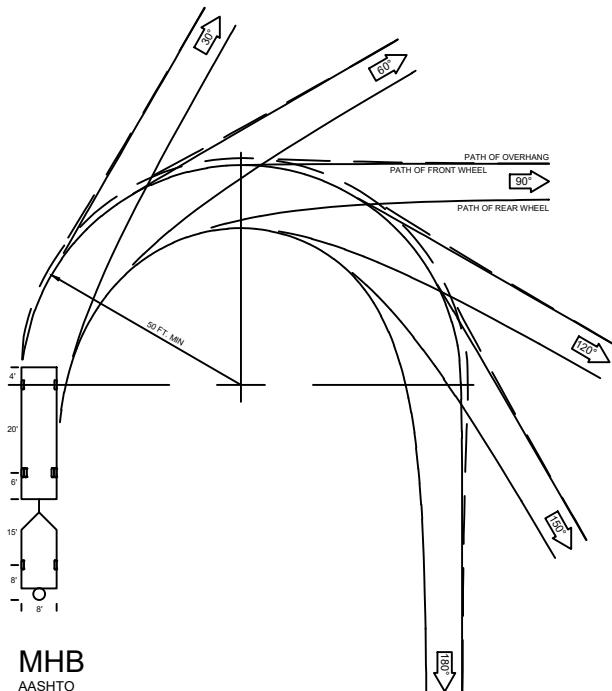


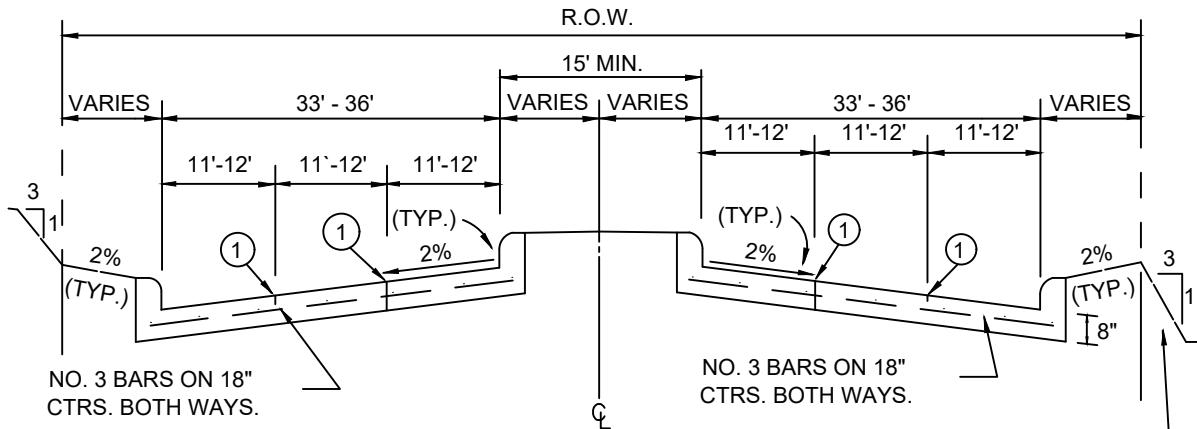
WB-67
AASHTO



MHB
AASHTO

M* - CITY OF MELISSA REVISION

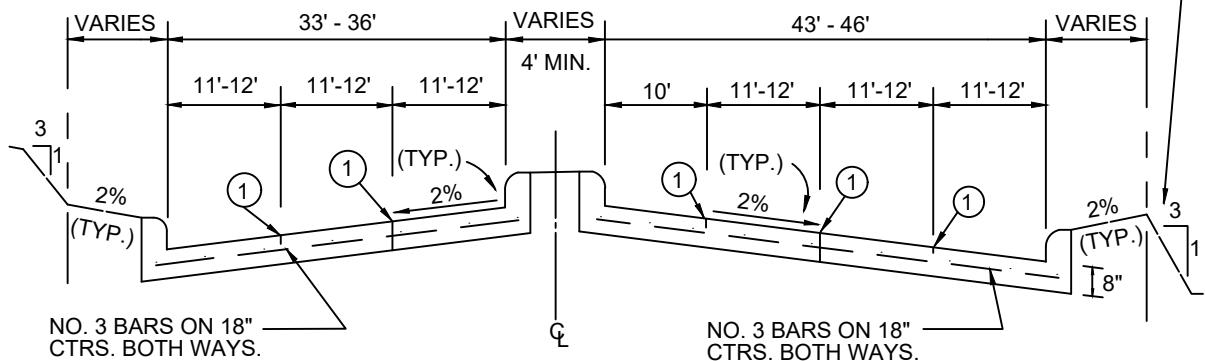
CURB RETURN TEMPLATES		NCTCOG STANDARD SPECIFICATION REFERENCE	
CITY OF MELISSA		DATE AUG. 04	STANDARD DRAWING NO. 2005M*



REGULAR SECTION

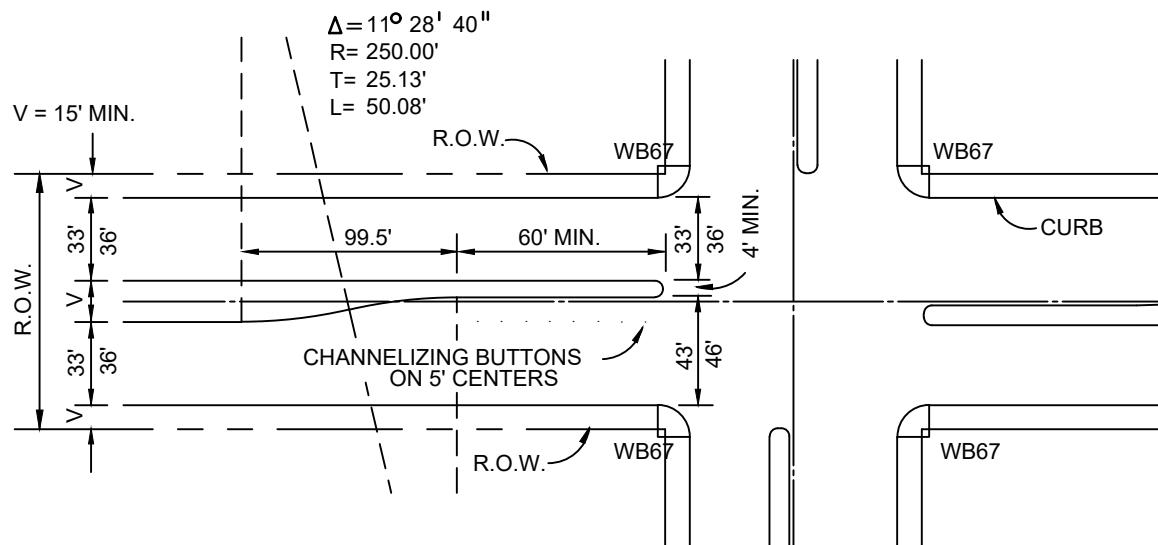
N.T.S.

(FILL SECTIONS ONLY. ALTERNATE REVERSE
SLOPE ACCEPTABLE. NOT TO EXCEED 3:1)



LEFT TURN SECTION

N.T.S.



NOTES:

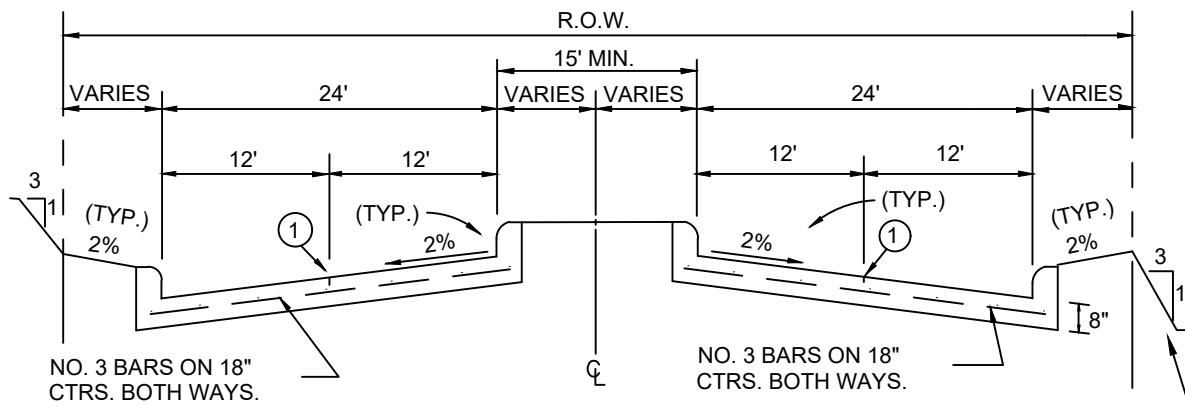
1. MIN. PAVEMENT DEPTH AND STRENGTH SHALL BE 8" - CLASS "C", OR AS SPECIFIED BY OWNER.
2. MIN. CURB HEIGHT AND WIDTH SHALL BE 6", OR AS SPECIFIED BY OWNER.
3. ALTERNATE REINFORCEMENT SHALL BE #4 BARS ON 24" CENTERS BOTH WAYS.

PLAN

N.T.S.

① SAWED LONGITUDINAL
CONTRACTION JOINT
OR CONSTRUCTION JOINT.

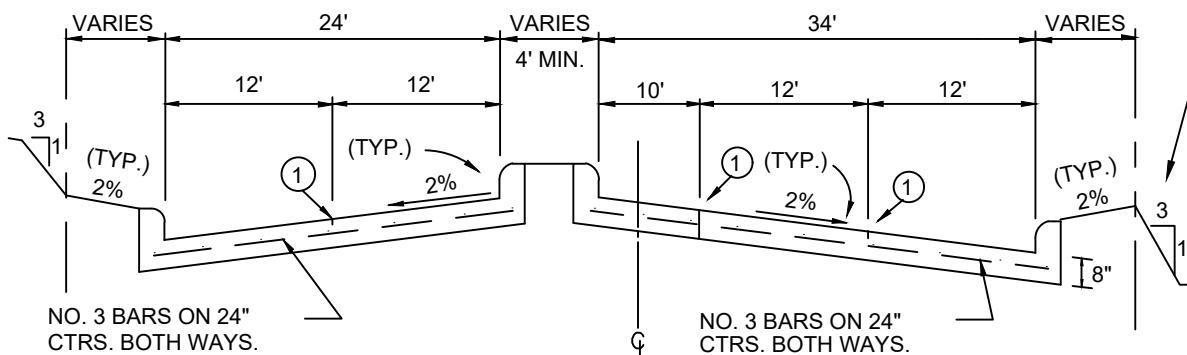
M* - CITY OF MELISSA REVISION



REGULAR SECTION

N.T.S.

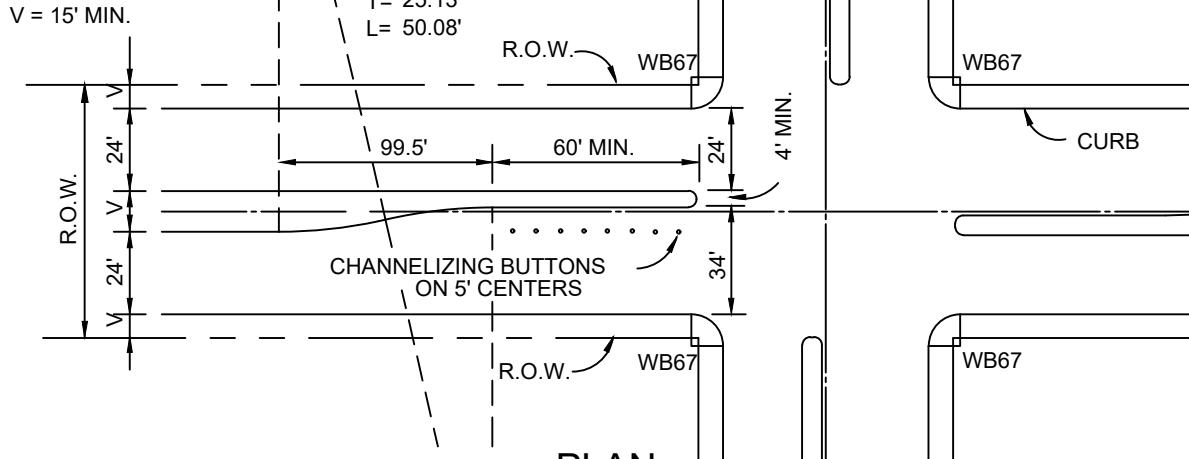
(FILL SECTIONS ONLY. ALTERNATE
REVERSE SLOPE ACCEPTABLE.
NOT TO EXCEED 3:1.)



LEFT TURN SECTION

N.T.S.

$\Delta = 11^{\circ} 28' 40''$
 $R = 250.00'$
 $T = 25.13'$
 $L = 50.08'$



NOTES:

1. MIN. PAVEMENT DEPTH AND STRENGTH SHALL BE 8" - CLASS "C", OR AS SPECIFIED BY OWNER.
2. MIN. CURB HEIGHT AND WIDTH SHALL BE 6", OR AS SPECIFIED BY OWNER.
3. ALTERNATE REINFORCEMENT SHALL BE #4 BARS ON 24" CENTERS BOTH WAYS.

(1) SAWED LONGITUDINAL
CONTRACTION JOINT
OR CONSTRUCTION JOINT.

M* - CITY OF MELISSA REVISION

NCTCOG STANDARD SPECIFICATION REFERENCE

303

REINFORCED CONCRETE PAVEMENT

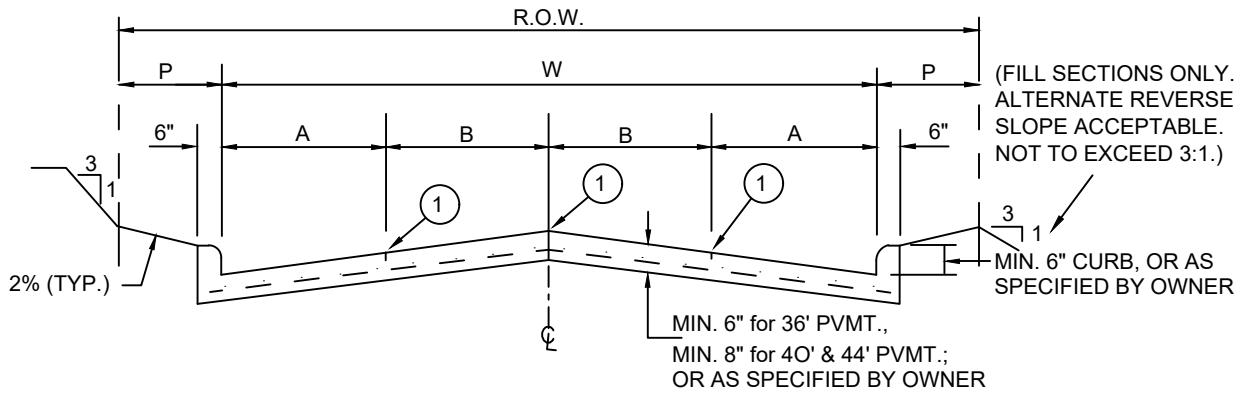
FOUR-LANE DIVIDED THOROUGHFARE



11/13/08

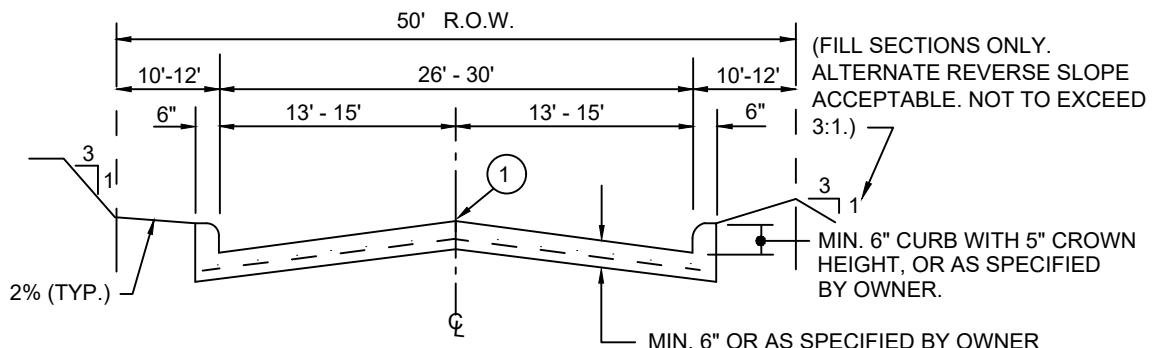
2020M*

STANDARD DRAWING NO.



STREET WIDTH(W)	A	B	R.O.W. WIDTH	P	CROWN HEIGHT
36'	8'	10'	VARIES	VARIES	6"
40'	8' OR 10'	10' OR 12'	VARIES	VARIES	6"
44'	11'	11'	VARIES	VARIES	8"

FOUR TRAVEL LANES OR
TWO TRAVEL LANES & TWO PARKING LANES
 N.T.S.



ONE TRAVEL LANE & TWO PARKING LANES
 N.T.S.

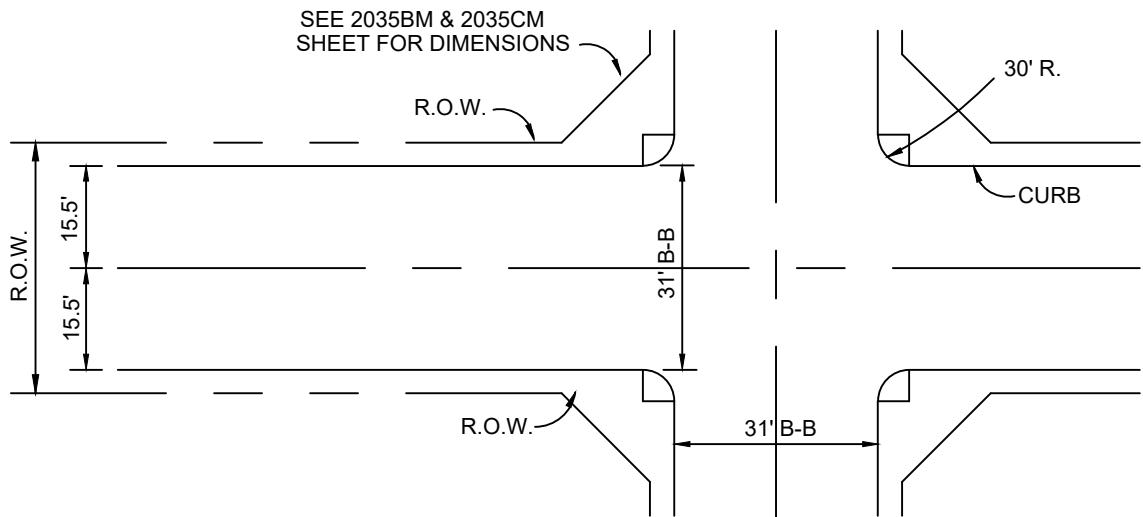
① INDICATES SAWED LONGITUDINAL CONTRACTION
 OR CONSTRUCTION JOINT.

NOTES :

1. ALL REINFORCEMENT SHALL BE #3 BARS ON 18" CENTERS BOTH WAYS, UNLESS OTHERWISE SPECIFIED BY OWNER.
2. PAVEMENT STRENGTH SHALL BE A MINIMUM OF 3600 PSI CONCRETE, OR AS SPECIFIED BY THE OWNER.

M - CITY OF MELISSA REVISION

REINFORCED CONCRETE PAVEMENT			APPLIED DATE	STANDARD SPECIFICATION REFERENCE	
2- & 4-LANE UNDIVIDED			08/29/19	303	
NOTICE DATE	ENFORCED DATE	STANDARD DRAWING NO.	08/29/19	09/29/19	2030M



ROAD TYPE	R.O.W.
RESIDENTIAL	50'
COLLECTOR	60'
THOROUGHFARE	100'
ARTERIAL	120'

RESIDENTIAL TO RESIDENTIAL

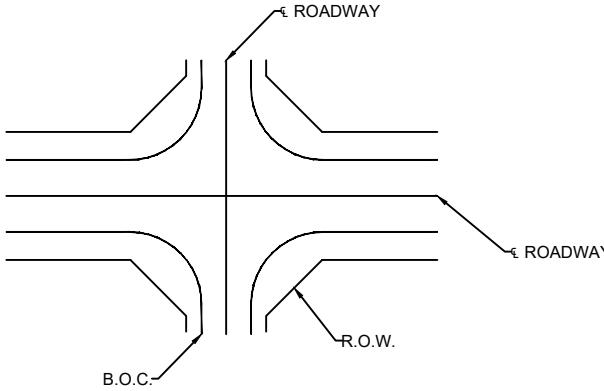
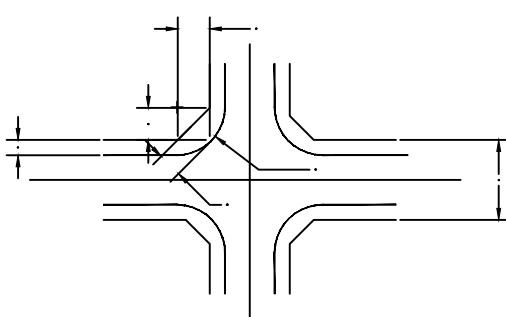
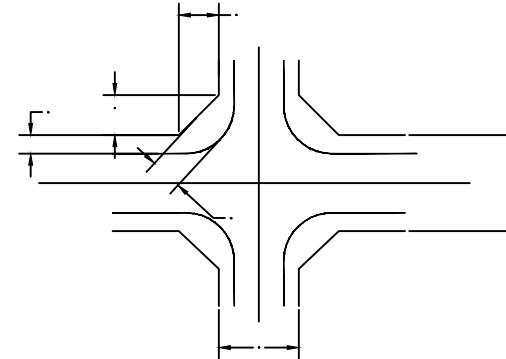
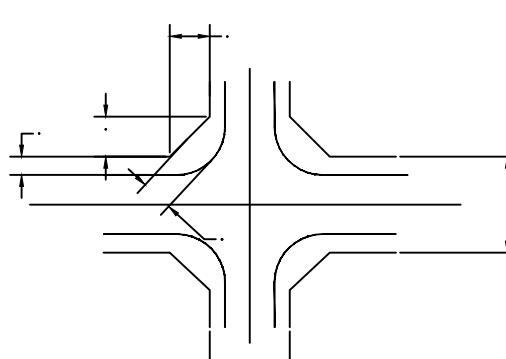
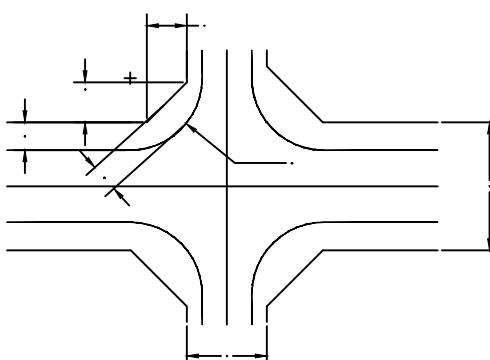
N.T.S.

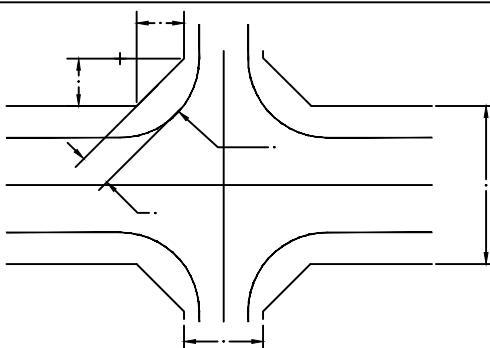
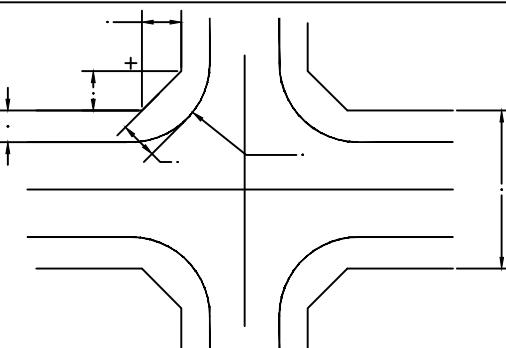
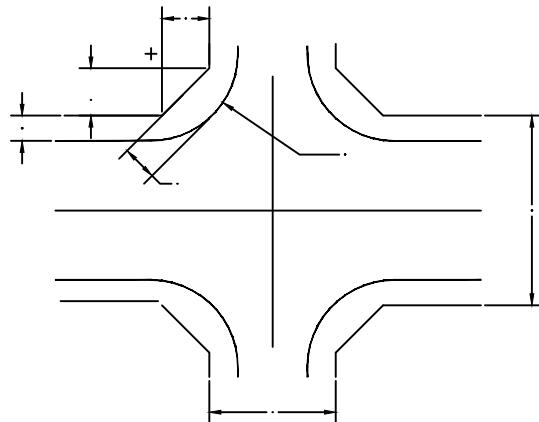
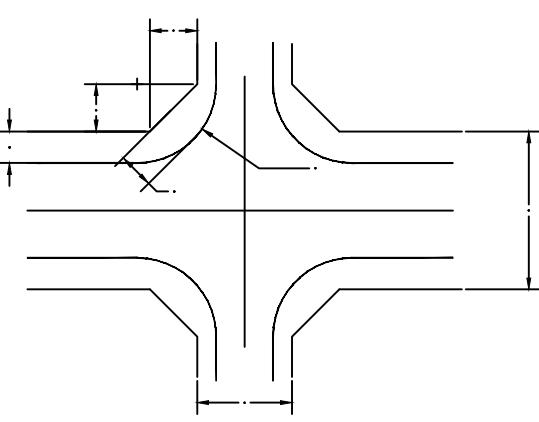
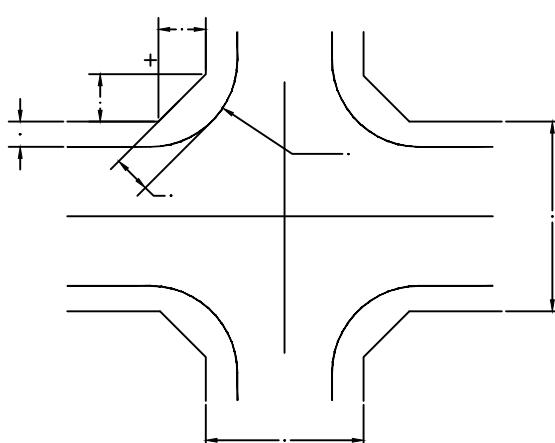
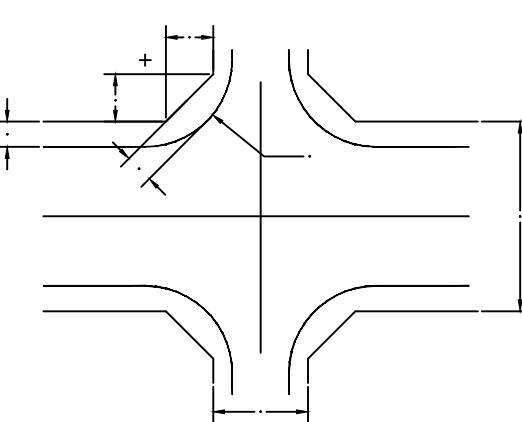
TYPE OF INTERSECTION	TEMPLATE OR RADIUS	MIN. R.O.W. CLIP
RESIDENTIAL TO RESIDENTIAL -	30'	20' X 20'
RESIDENTIAL TO COLLECTOR -	MHB	20' X 20'
RESIDENTIAL TO THOROUGHFARE -	WB67	30' X 30'
RESIDENTIAL TO ARTERIAL -	NOT ALLOWED	NA
COLLECTOR TO THOROUGHFARE -	WB67	30' X 30'
COLLECTOR TO ARTERIAL -	WB67	30' X 30'
THOROUGHFARE TO ARTERIAL -	WB67	30' X 30'

M* - CITY OF MELISSA REVISION

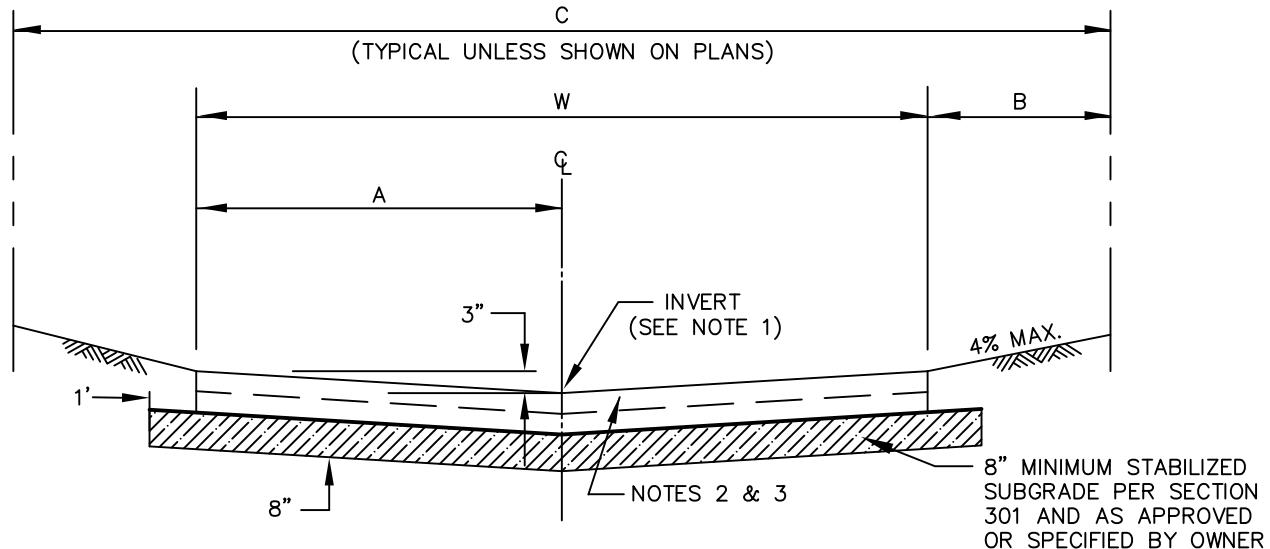
REINFORCED CONCRETE PAVEMENT		NCTCOG STANDARD SPECIFICATION REFERENCE	
		MODIFIED DATE	STANDARD DRAWING NO.
		02/5/14	2035AM*
INTERSECTION ROW AND CURB RETURN DETAILS		NOTICE DATE	ADOPTED DATE
			ENFORCEMENT DATE



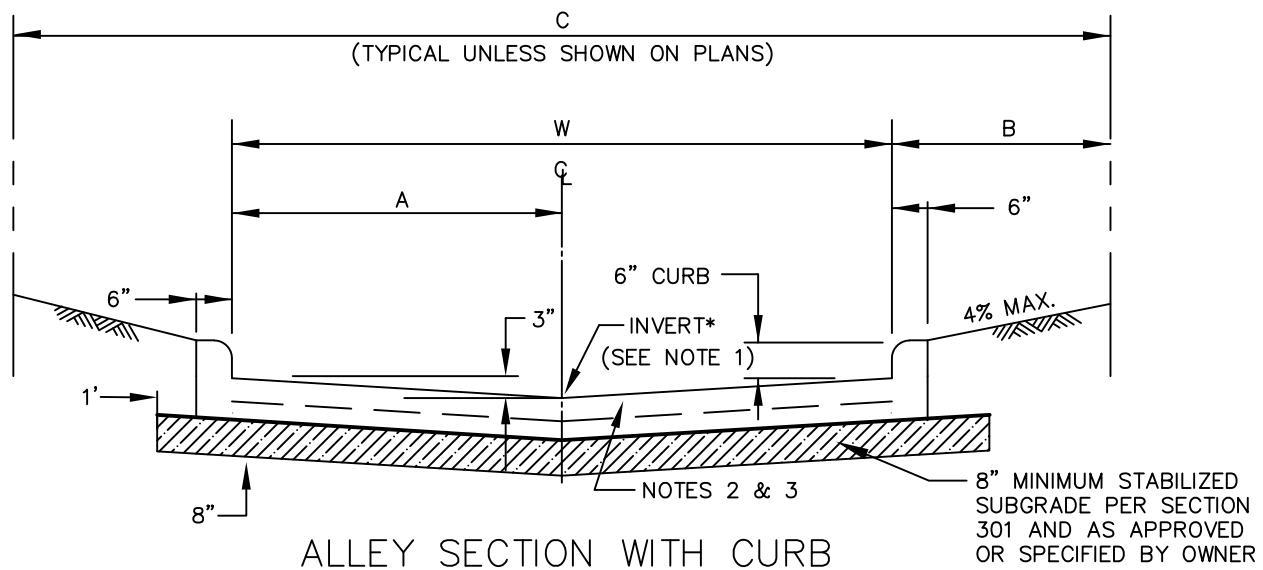
 <p>ROADWAY</p> <p>R.O.W.</p> <p>B.O.C.</p>	
 <p>RESIDENTIAL TO RESIDENTIAL</p>	 <p>RESIDENTIAL TO MINOR COLLECTOR</p>
 <p>ESTATE RESIDENTIAL TO MINOR COLLECTOR (K2U) (C3U)</p>	 <p>RESIDENTIAL TO MAJOR COLLECTOR (R2U, R3U) (C4U)</p>
<p>NOTE: ALL DIMENSIONS IN FEET</p> <p>INTERSECTION ROW AND CURB RETURN DETAILS</p> <p>CITY OF MELISSA</p>	
<p>M* - CITY OF MELISSA REVISION</p> <p>NCTCOG STANDARD SPECIFICATION REFERENCE</p> <p>MELISSA TX</p> <p>MODIFIED DATE 02/5/14</p> <p>STANDARD DRAWING NO. 2035BM*</p>	
<p>NOTICE DATE</p> <p>ADOPTED DATE</p> <p>ENFORCEMENT DATE</p>	

 <p>RESIDENTIAL TO THOROUGHFARE (R2U, R3U) (M4U-M7U)</p>	 <p>MAJOR COLLECTOR TO THOROUGHFARE (C4U) (M4U-M7U)</p>
 <p>MAJOR COLLECTOR TO ARTERIAL (C4U) (M4U-M7U)</p>	 <p>MINOR COLLECTOR TO THOROUGHFARE (C3U) (M4U-M7U)</p>
 <p>THOROUGHFARE TO ARTERIAL (M4U-M7U) (M4U-M7U)</p>	 <p>MINOR COLLECTOR TO ARTERIAL (C3U) (M4U-M7U)</p>
<p>NOTE: ALL DIMENSIONS IN FEET</p>	
<p>INTERSECTION ROW AND CURB RETURN DETAILS</p>	
<p>CITY OF MELISSA</p>	

 MELISSA		M* - CITY OF MELISSA REVISION NCTCOG STANDARD SPECIFICATION REFERENCE
MODIFIED DATE 02/5/14	STANDARD DRAWING NO. 2035CM*	
NOTICE DATE	ADOPTED DATE ENFORCEMENT DATE	



ALLEY SECTION WITHOUT CURB
N.T.S.

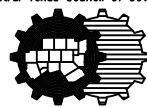


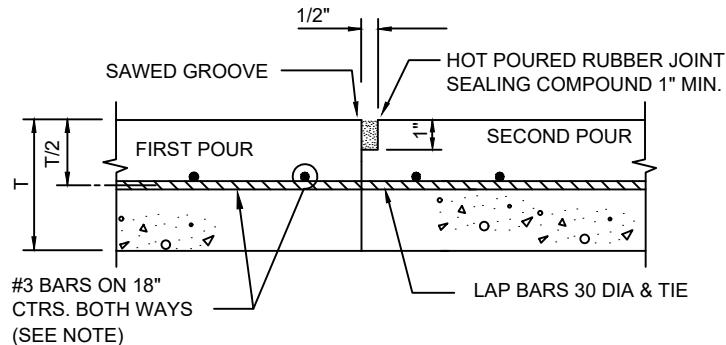
ALLEY SECTION WITH CURB
N.T.S.

NOTES:

1. CROWN SECTION MAY BE USED IN LIEU OF INVERT WITH PROVISION OF AN ADEQUATE DRAINAGE DESIGN AND AS APPROVED BY OWNER.
2. REINFORCED WITH NO. 3 BARS AT 18" C-C BOTH WAYS OR AS APPROVED BY OWNER.
3. ALTERNATIVE SUBGRADE, THICKNESS, AND STEEL MAY BE UTILIZED WITH MORE DETAILED STUDY AND ANALYSIS AND AS APPROVED BY OWNER.
4. EXPANSION JOINTS TO BE PLACED AT INTERSECTION AND NOT TO EXCEED 600' BETWEEN JOINTS.
5. CONCRETE SHALL BE CLASS "C" OR "PC", OR AS SPECIFIED BY OWNER.
6. SEE DETAIL 2170 FOR SIDEWALKS.

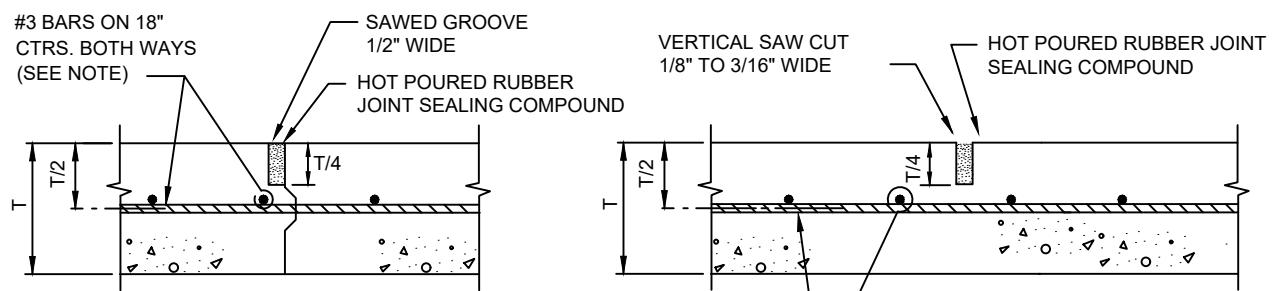
ALLEY WIDTH (W)	A	B	R.O.W. WIDTH (C)
10'	5'	2'-6"	15'
12'	6'	2'-6"	17'
16'	8'	2'-6"	21'
20'	10'	2'-6"	25'





CONSTRUCTION JOINT

N.T.S.



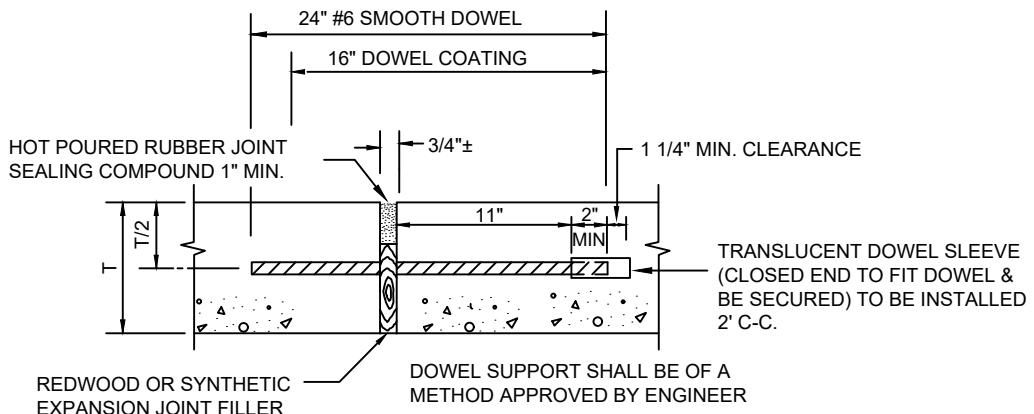
KEYWAY JOINT

(FOR PAVEMENT THICKNESS > 6")
N.T.S.

SAWED CONTRACTION JOINT

N.T.S.

NOTE:
ALTERNATE REINFORCEMENT
#4 BARS ON 24" CTRS.
BOTH WAYS.



EXPANSION JOINT

(SPACED 600 FT. MAXIMUM; LOCATE AT
STRUCTURES AND AT INTERSECTION P.C.'S & P.T.'S)
N.T.S.

REINFORCED CONCRETE PAVEMENT JOINTS

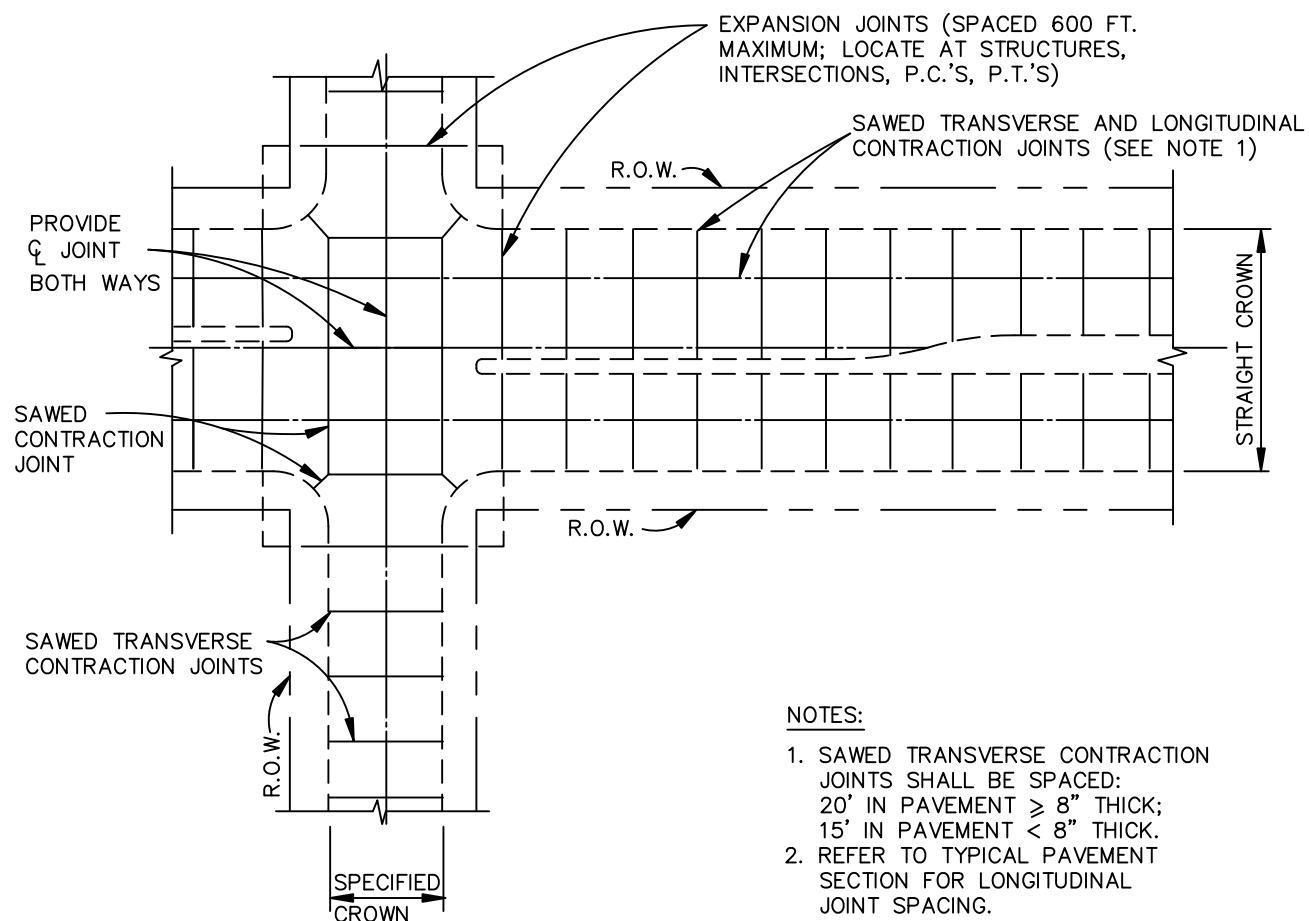
STANDARD SPECIFICATION REFERENCE

303.5.4



DATE
11/26/19

STANDARD DRAWING NO.
2050



SPACING DIAGRAM FOR TRANSVERSE JOINTS

N.T.S.

REINFORCED CONCRETE PAVEMENT

TRANSVERSE JOINT SPACING

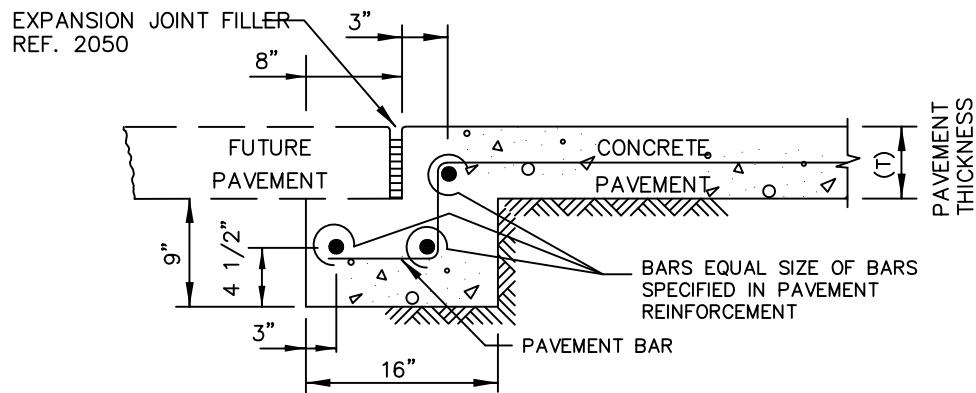
North Central Texas Council of Governments



STANDARD SPECIFICATION REFERENCE

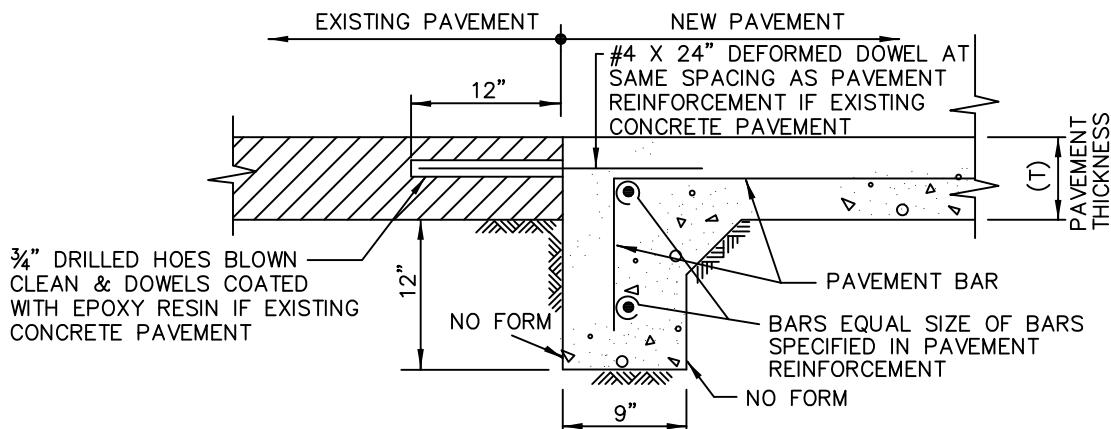
303.5.4.

DATE	STANDARD DRAWING NO.
AUG '23	2060



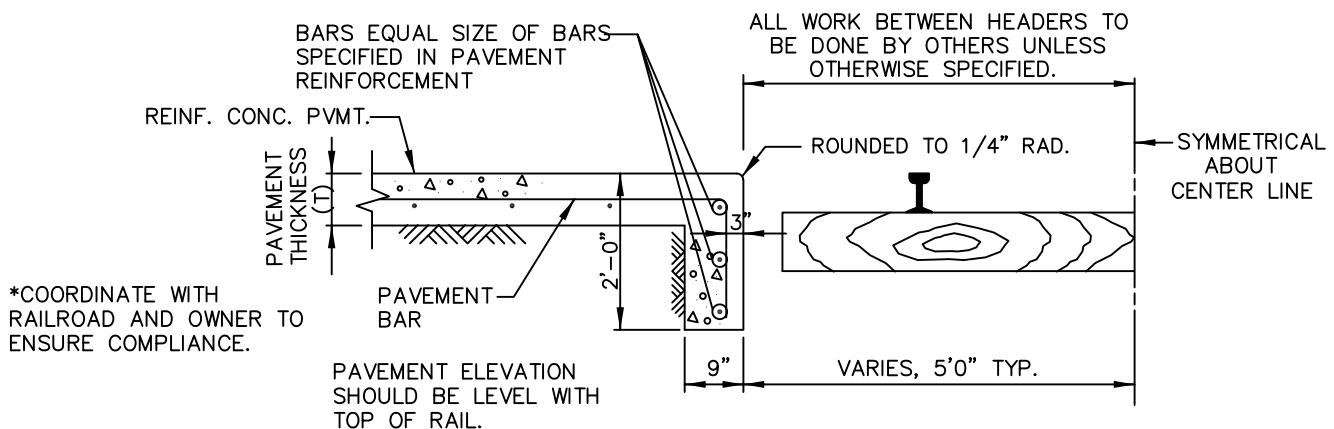
STREET HEADER FOR FUTURE PAVEMENT

N.T.S.



STREET HEADER AT EXISTING PAVEMENT

N.T.S.



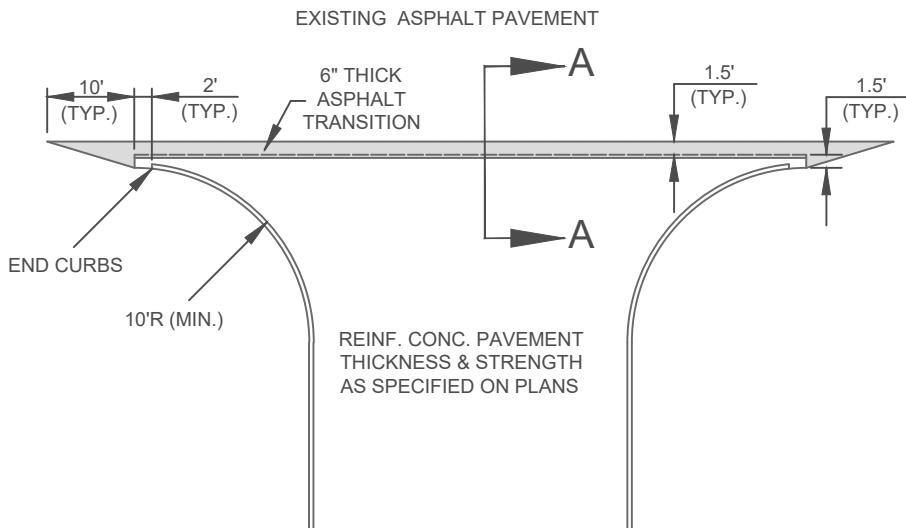
NOTES:

1. PAVEMENT BARS TO BE BENT DOWN INTO HEADER.

2. HEADER AND PAVEMENT TO BE MONOLITHIC.

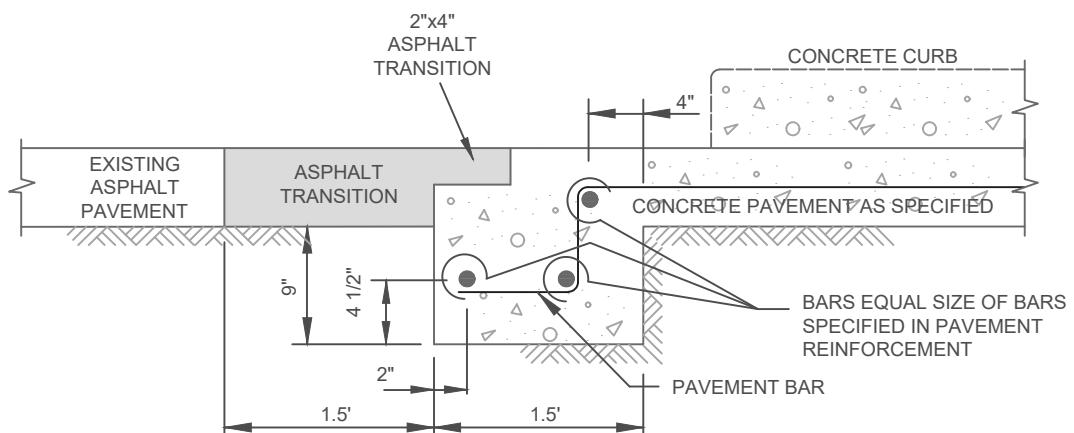
STREET HEADER AT RAILROAD

N.T.S.



CONCRETE TO ASPHALT CONNECTION

N.T.S.



SECTION A-A

N.T.S.

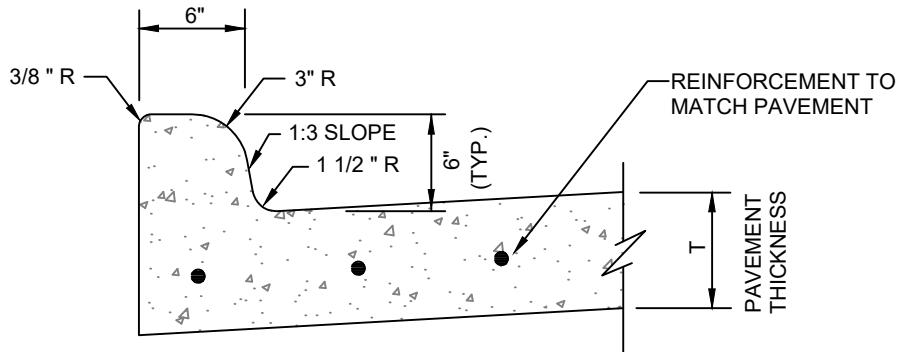
M* - CITY OF MELISSA REVISION

CONCRETE TO ASPHALT PAVEMENT CONNECTION		MELISSA TX	NCTCOG STANDARD SPECIFICATION REFERENCE 305.4
DATE 11/13/08	STANDARD DRAWING NO. 2070AM*		

GENERAL NOTES:

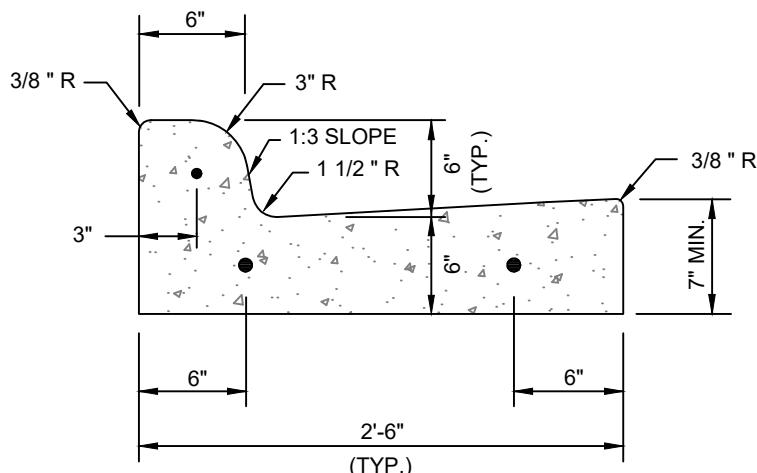
1. REINFORCED CONCRETE PAVEMENT:
 - A. ALL CURBS SHALL BE PLACED INTEGRAL WITH PAVEMENT UNLESS OTHERWISE APPROVED BY THE OWNER.
 - B. CURBS SHALL MEET THE SAME COMPRESSIVE STRENGTH AS SPECIFIED FOR THE PAVEMENT.
 - C. BAR LAPS SHALL BE 30 DIAMETERS.
 - D. REINFORCING BARS SHALL BE SUPPORTED BY CHAIRS OR OTHER DEVICES APPROVED BY THE OWNER.
2. SUBGRADE: (UNLESS OTHERWISE SPECIFIED BY OWNER)
 - A. SUBGRADE UNDER ALL PAVEMENTS SHALL BE STABILIZED TO MINIMUM DEPTH OF 6" FOR 2 LANE RESIDENTIAL ROAD AND 8" FOR ALL OTHERS. IF THE P.I. IS 15 OR GREATER, LIME SHALL BE USED, IF THE P.I. IS LESS THAN 15, CEMENT SHALL BE USED OR AS RECOMMENDED BY A GEOTECH ENGINEER. LABORATORY TESTS MUST BE PERFORMED TO DETERMINE THE AMOUNT OF CEMENT REQUIRED TO LOWER THE P.I. TO 15 OR BELOW SATURATION P.I. (PH \geq 12.4) WILL BE THE LIMIT WHEN A SOIL'S P.I. CANNOT BE BROUGHT TO 15 OR LOWER.
 - B. WHERE THE IN PLACE MATERIAL HAS A P.I. OF LESS THAN 15, THE SUBGRADE SHALL BE SCARIFIED TO A MINIMUM DEPTH OF 6" AND RECOMPACTED.
 - C. WHERE SULFATES ARE PRESENT, CONSULT A GEOTECHNICAL ENGINEER FOR RECOMMEND SUBGRADE TREATMENT.
3. IF THE ROADWAY IS A DESIGNATED BIKE ROUTE OR BIKE USAGE IS ANTICIPATED, REFER TO AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS AASHTO GUIDE FOR THE DEVELOPMENT OF BICYCLE FACILITIES (2012, 4TH EDITION) AND THE TEXAS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (TMUTCD):
https://mutcd.fhwa.dot.gov/resources/state_info/texas/tx.htm

PAVEMENT SYSTEMS		STANDARD SPECIFICATION REFERENCE	
GENERAL NOTES		301. 302. 303.	STANDARD DRAWING NO.
		DATE AUG '23	2110



INTEGRAL CURB & GUTTER

N.T.S.



SEPARATE CURB & GUTTER

N.T.S.

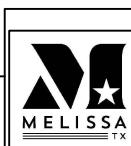
NOTES:

1. REINFORCEMENT SHALL BE NO. 4 BARS, UNLESS OTHERWISE SPECIFIED.
2. CONCRETE SHALL BE CLASS "C" OR "PC", 3,600 PSI, UNLESS OTHERWISE SPECIFIED.
3. ALL CURBS SHALL BE CONSTRUCTED OF PORTLAND CEMENT CONCRETE UNLESS OTHERWISE SPECIFIED.
4. REBAR STAKES SHALL BE USED IN RAISED CURB SECTIONS FOR HAND PLACED CONCRETE.

M* - CITY OF MELISSA REVISION

NCTCOG STANDARD SPECIFICATION REFERENCE

305.1



MODIFIED DATE

07/28/23

STANDARD DRAWING NO.

2120M

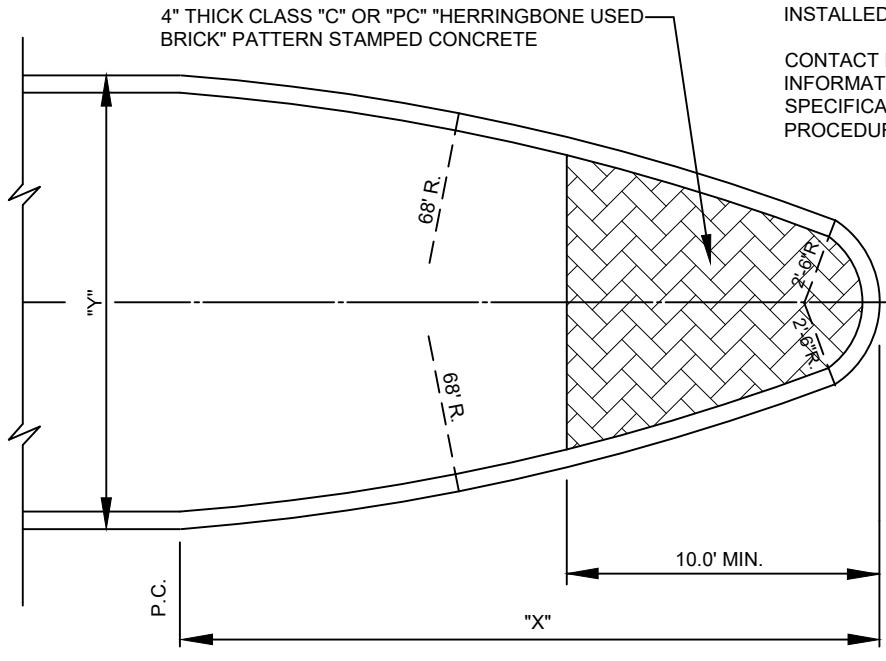
CONCRETE CURB & GUTTER

INTEGRAL & SEPARATE

NOTICE DATE
07/28/23

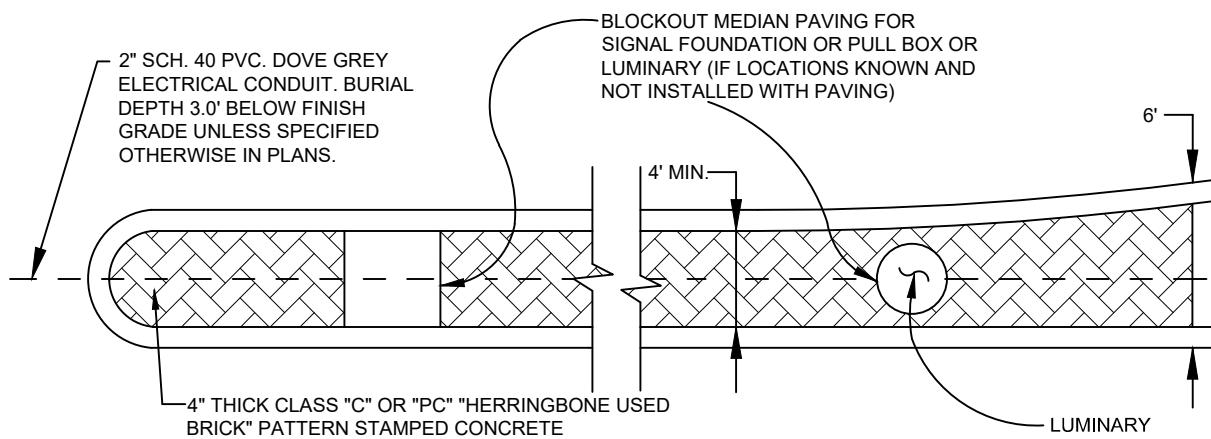
ADOPTED DATE
07/28/23

ENFORCEMENT DATE
08/28/23



CONCRETE NOSE FOR MEDIAN ISLAND N.T.S.

NOTE: MEDIAN PAVING SHALL EXTEND TO POINT WHERE MEDIAN IS 6' WIDE. IF MEDIAN IS 6' WIDE, PAVING SHALL EXTEND 15' FROM NOSE. FOR MEDIAN WIDER THAN 6' PAVING SHALL EXTEND 10' FROM NOSE. ALL DISTANCES ARE MINIMUM.



LEFT TURN LANE MEDIAN PAVEMENT

N.T.S.

MEDIAN ISLAND PAVEMENT

NOSE & LEFT TURN LANE

M* - CITY OF MELISSA REVISION

305.3



NCTCOG STANDARD SPECIFICATION REFERENCE

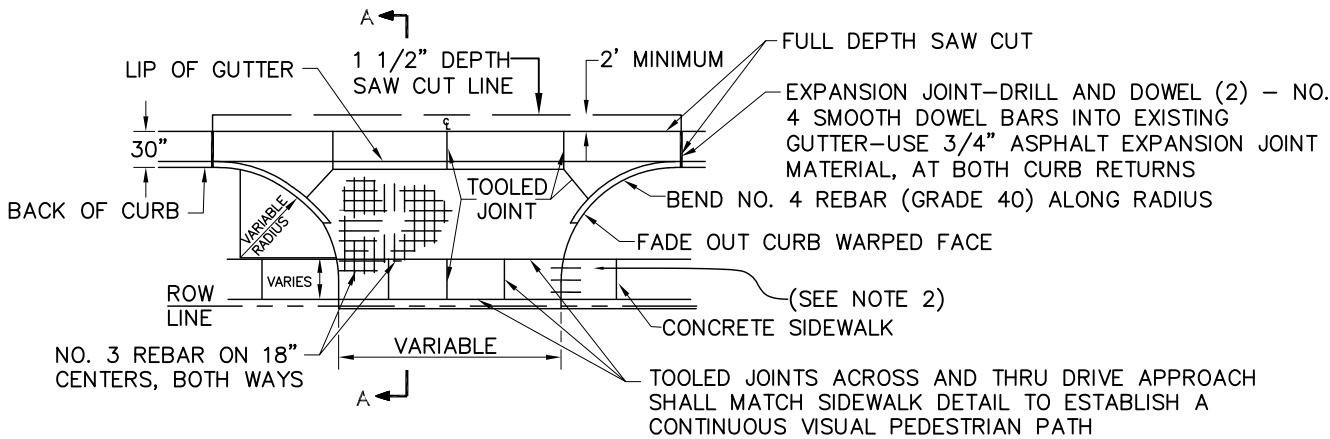
MODIFIED DATE

STANDARD DRAWING NO.
2130M*

NOTICE DATE
02/28/17

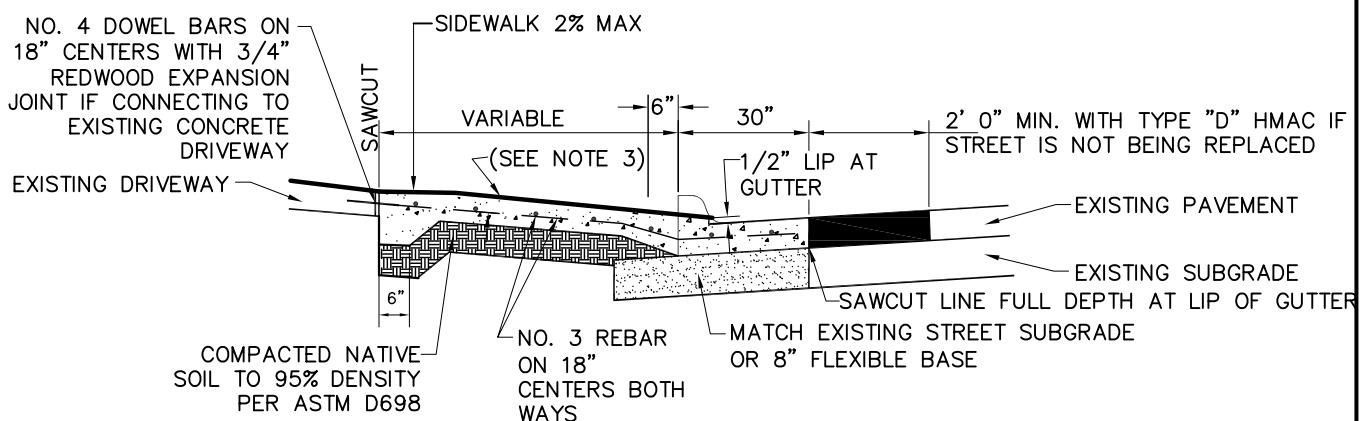
ADOPTED DATE
03/30/17

ENFORCEMENT DATE
03/30/17



TYPICAL DRIVE APPROACH CONNECTING TO ASPHALT STREETS WITH CURB AND GUTTER

N.T.S.

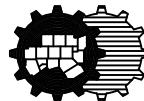


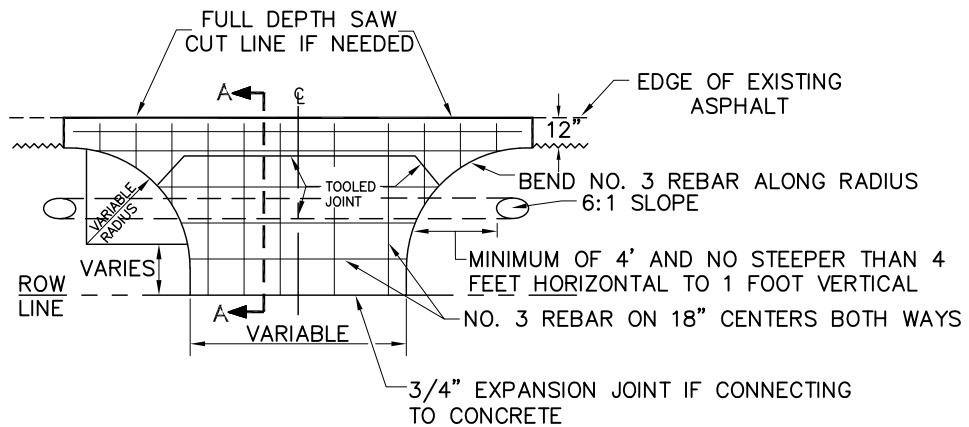
SECTION 'A-A'

N.T.S.

NOTES:

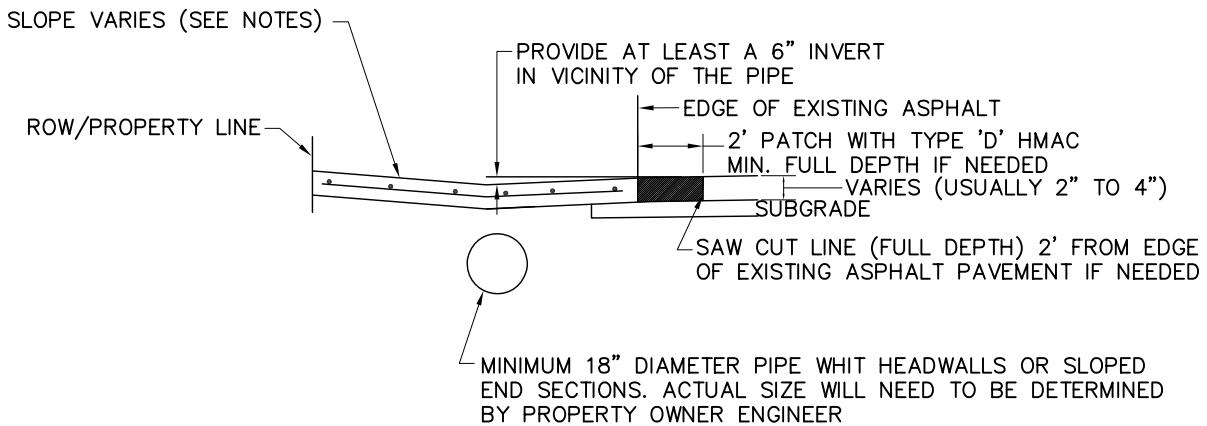
1. THE SLOPE OF THE DRIVE WHERE SIDEWALKS CROSS SHALL HAVE A MAXIMUM CROSS SLOPE OF 2%.
2. REMOVE ANY EXISTING SIDEWALK AT NEAREST JOINT AND CONNECT REPLACED SECTION TO DRIVE WITH (3) - NO. 4 SMOOTH DOWEL BARS ON 18" CENTERS WITH 3/4" REDWOOD EXPANSION JOINT, WITH 1" REMOVABLE CAP STRIP. SEAL WITH SELF LEVELING GRAY SILICONE SEALANT.
3. RESIDENTIAL DRIVE APPROACH 10% MAX SLOPE*, MIN. 5" SLAB THICKNESS
* MAXIMUM SLOPE AS APPROVED BY OWNER
4. ALL CONNECTIONS TO STATE RIGHT-OF-WAY SHALL USE TXDOT DETAILS.
5. ALL CURB AND GUTTER SHALL BE 30" UNLESS OTHERWISE DIRECTED BY THE CITY.
6. CONCRETE SHALL BE CLASS C, 6 SACK AND HAVE COMPRESSIVE STRENGTH OF 3600 PSI @28 DAYS.
7. IF STREET IS BEING REPLACED, PAVEMENT THICKNESS SHALL BE 6" FOR RESIDENTIAL AND 8" FOR COLLECTOR OR LARGER.





TYPICAL DRIVE APPROACH CONNECTING TO EXISTING
RURAL TYPE ASPHALT STREETS

N.T.S.



SECTION 'A-A'

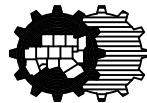
N.T.S.

NOTES:

1. THE SLOPE OF THE DRIVE WHERE SIDEWALKS CROSS SHALL HAVE A MAXIMUM CROSS SLOPE OF 2%.
2. RESIDENTIAL DRIVE APPROACH 10% MAX SLOPE*; MIN. 5" SLAB THICKNESS
* MAXIMUM SLOPE AS APPROVED BY OWNER
3. ALL CONNECTIONS TO STATE RIGHT-OF-WAY SHALL USE TXDOT DETAILS.
4. CONCRETE SHALL BE CLASS C, 6 SACK AND HAVE COMPRESSIVE STRENGTH OF 3600 PSI @ 28 DAYS.
5. MINIMUM VELOCITY THROUGH PIPE IS 2.5fps. MINIMUM SLOPE IN PIPE IS 0.5% UNLESS OTHERWISE DESIGNED TO MEET MINIMUM SLOPE REQUIREMENTS
6. IN SOME CASES A SWALE MAY BE PROVIDED IN LIEU OF THE PIPE. THE PROPERTY OWNER AND OWNER'S ENGINEERS WILL NEED TO DETERMINE IF A SWALE CAN BE USED IN LIEU OF A PIPE.
7. USE OF RURAL SECTION AS APPROVED BY OWNER.

RESIDENTIAL DRIVE APPROACH CONNECTING
TO EXISTING RURAL TYPE ASPHALT STREET

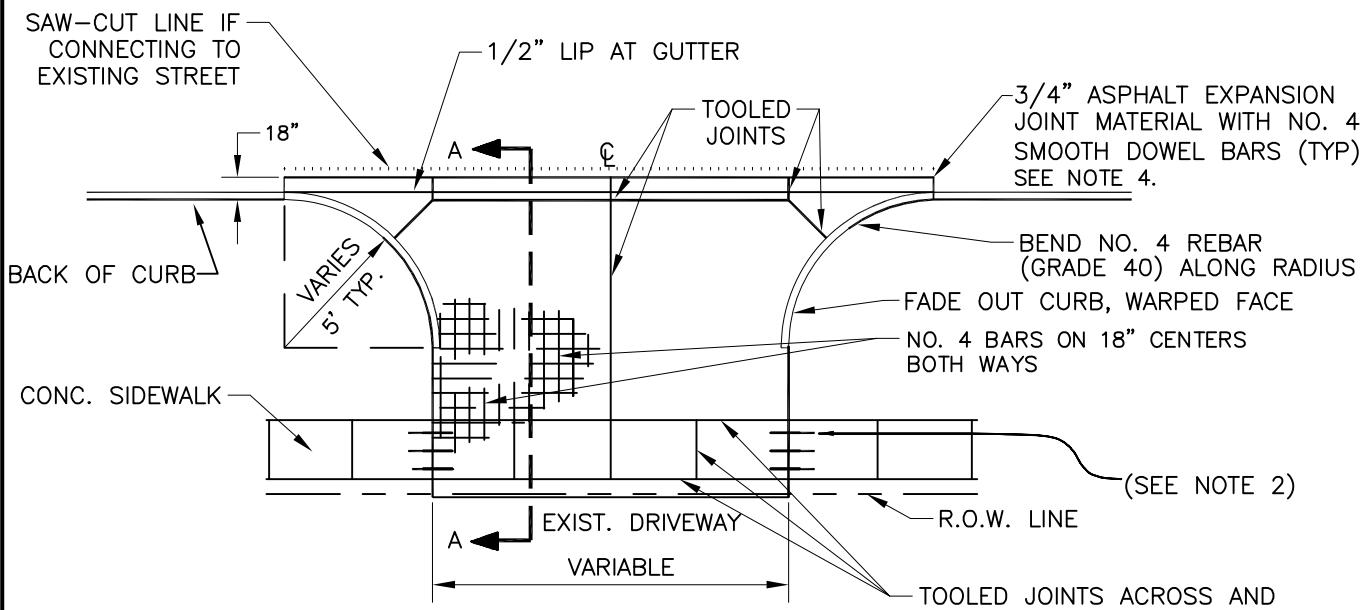
North Central Texas Council of Governments



STANDARD SPECIFICATION REFERENCE

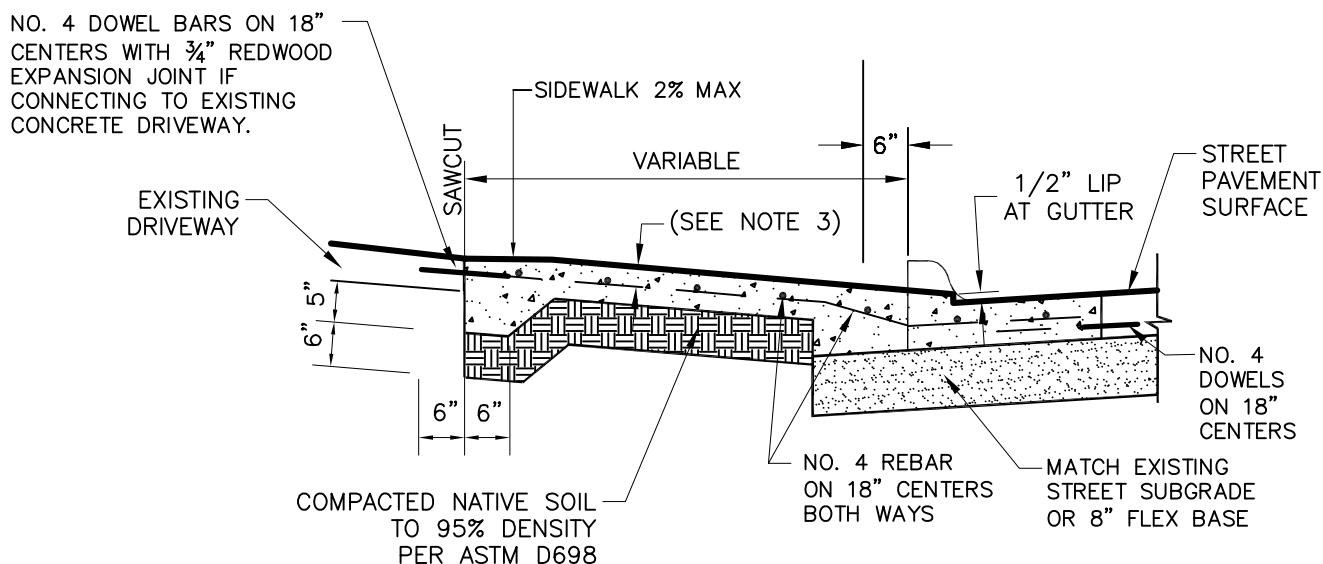
305.2

DATE AUG '23	STANDARD DRAWING NO. 2150B
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**TYPICAL DRIVE APPROACH
ON A CONCRETE STREET**

N.T.S.



SECTION 'A-A'

N.T.S.

SEE NOTES FOR 2150A

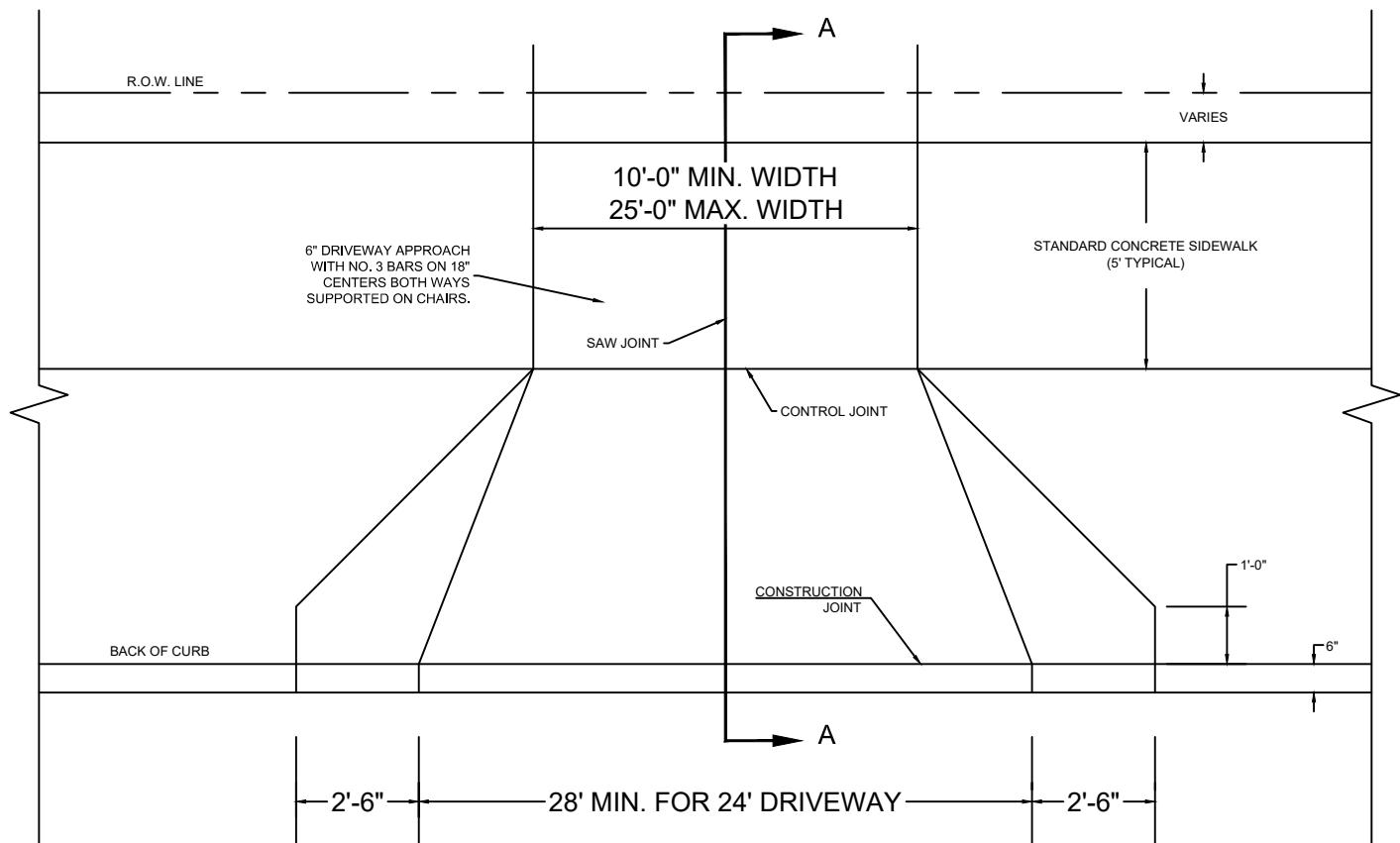
**RESIDENTIAL DRIVE APPROACH
ON A CONCRETE STREET**



STANDARD SPECIFICATION REFERENCE

305.2

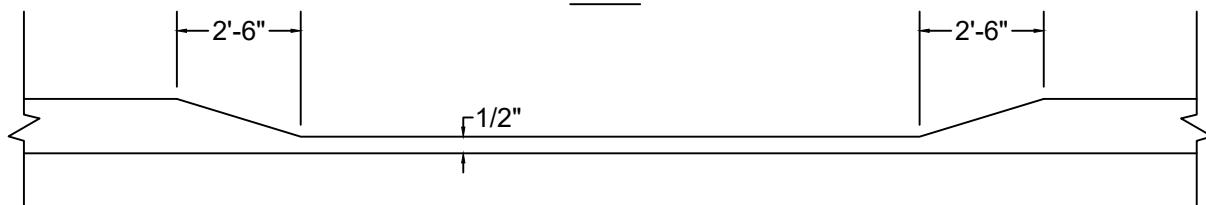
DATE	STANDARD DRAWING NO.
AUG '23	2150C



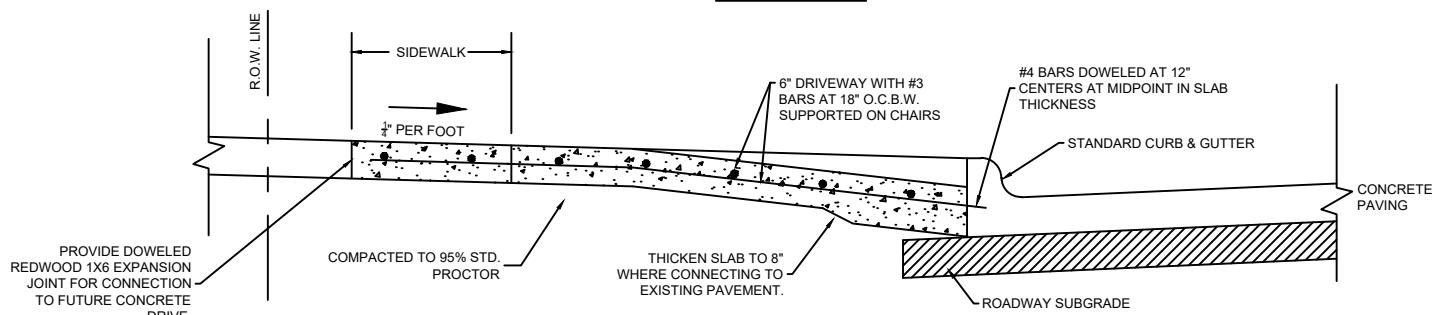
NOTE:

EXISTING CURB AND GUTTER, IF ANY, MUST BE SAWED AS DIRECTED BY THE CITY ENGINEER. HORIZONTAL CURB CUT SHALL BE MADE AT AN ELEVATION OF 1/2" ABOVE THE EXISTING GUTTER WITH A MINIMUM LENGTH AS SHOWN. THE TRADITIONAL SAW CUT SHALL HAVE A RUN OF 2'-6" AND SHALL RISE TO MEET THE EXISTING TOP OF CURB. ALL EXPOSED EDGES SHALL BE GROUNDED TO A 1/4" RADIUS. SAW CUTTING SHALL BE PERFORMED WITH A RIDE-ON SAW EQUIPPED WITH A DIAMOND SAW BLADE.

PLAN



ELEVATION

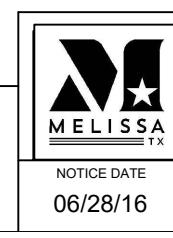


SECTION A-A
RESIDENTIAL APPROACH (CONCRETE PAVING)
N.T.S.

NOTE:
1. SIDEWALK SECTION THRU DRIVEWAY SHALL BE POURED SAME THICKNESS AS DRIVEWAY APPROACH. (EXISTING SIDEWALK, IF ANY, SHALL BE REMOVED & REPLACED).

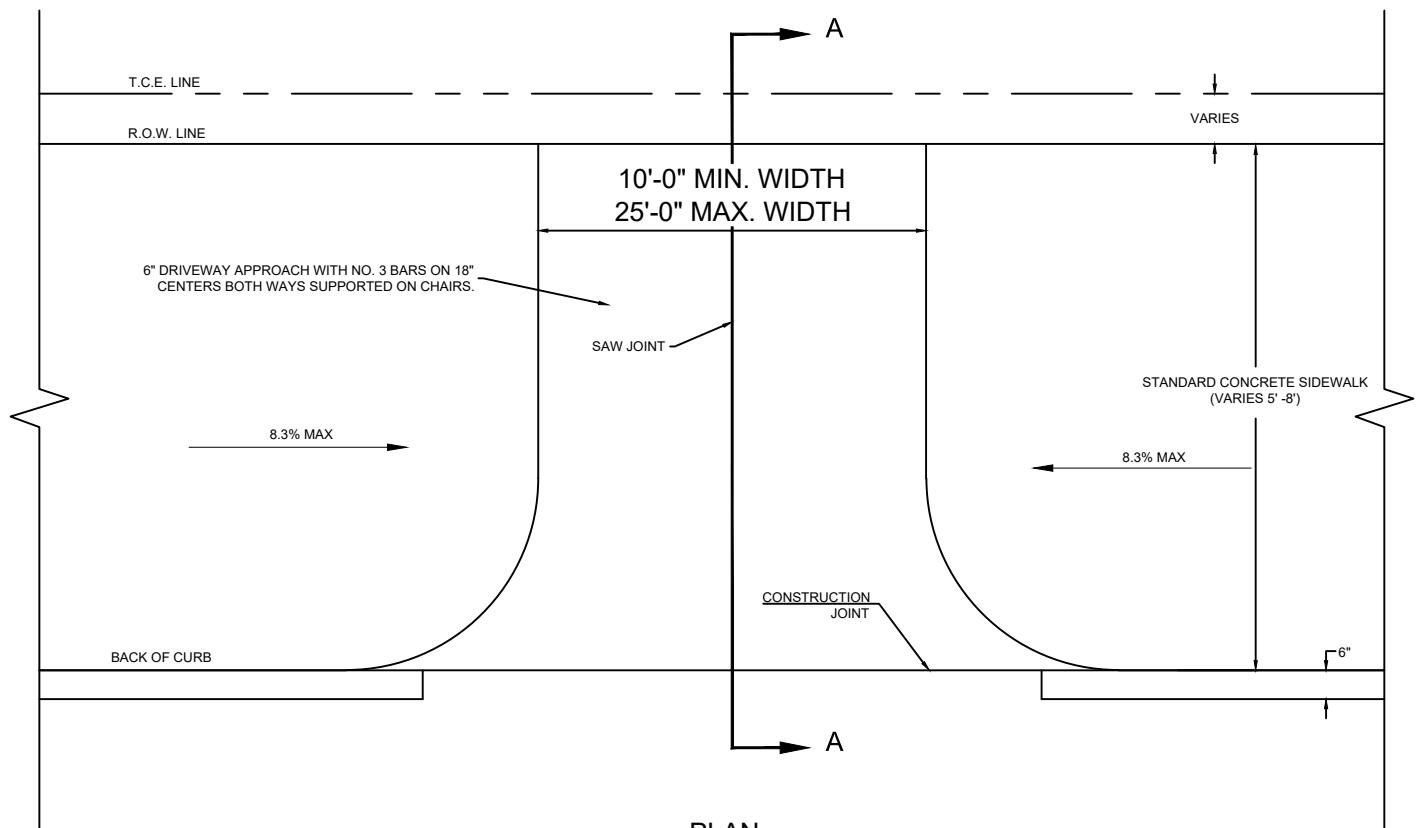
** THIS WORK SHALL NOT DISRUPT THE DESIGN FLOWLINE OF THE EXISTING GUTTER.

HORIZONTAL CURB CUT DETAIL

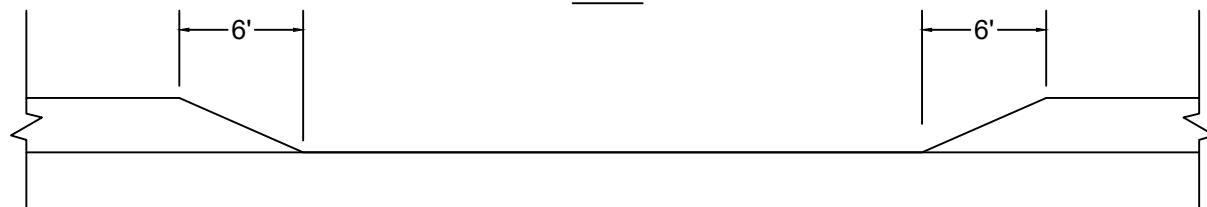


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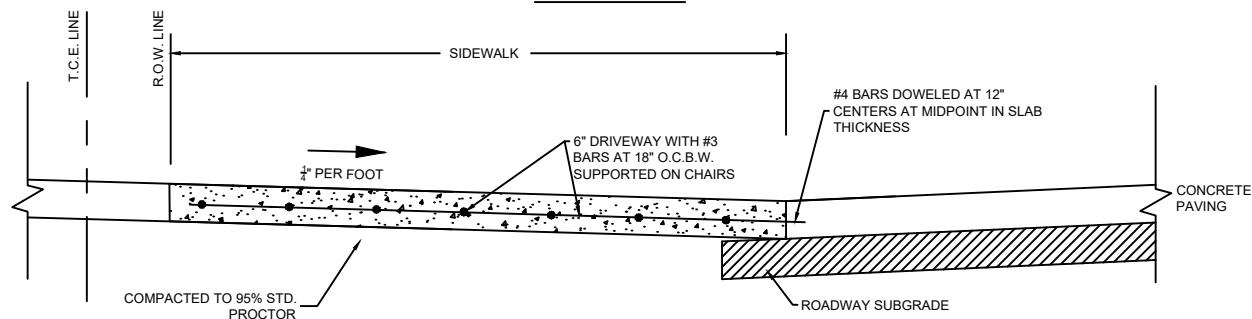
NCTCOG STANDARD SPECIFICATION REFERENCE	
MODIFIED DATE	STANDARD DRAWING NO.
06/28/16	2150CM*
NOTICE DATE	APPLIED DATE
06/28/16	07/28/16
ENFORCED DATE	
07/28/16	



PLAN



ELEVATION



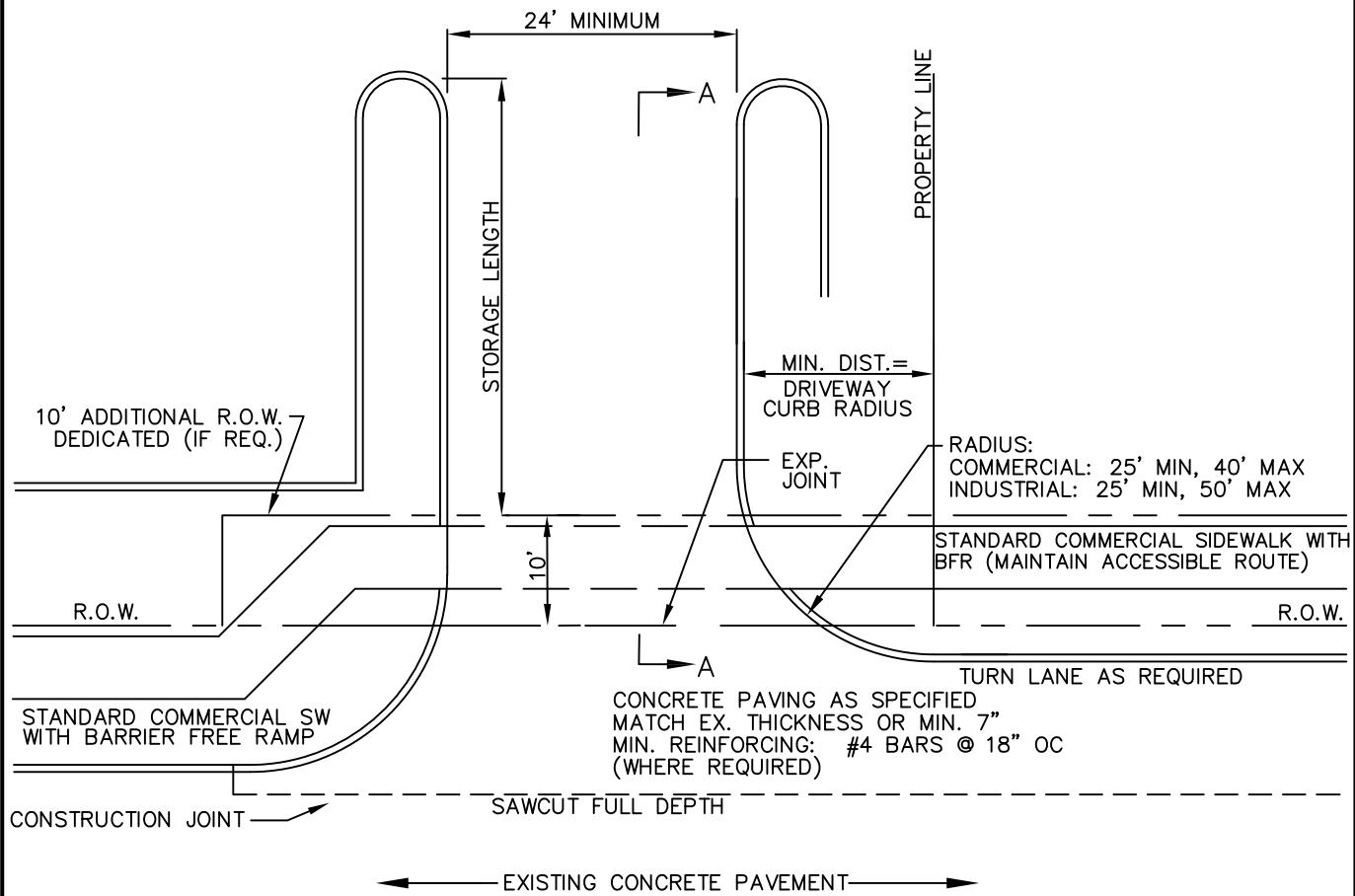
SECTION A-A
RESIDENTIAL APPROACH (CONCRETE PAVING)
N.T.S.

NOTE:
1. SIDEWALK SECTION THRU DRIVEWAY SHALL BE POURED SAME THICKNESS AS DRIVEWAY APPROACH. (EXISTING SIDEWALK, IF ANY, SHALL BE REMOVED & REPLACED).

** THIS WORK SHALL NOT DISRUPT THE DESIGN FLOWLINE OF THE EXISTING GUTTER.

DOD DRIVEWAY DETAIL

MELISSA TX		NCTCOG STANDARD SPECIFICATION REFERENCE
MODIFIED DATE		STANDARD DRAWING NO.
06/14/24	2150CM-DOD*	
NOTICE DATE	APPLIED DATE	ENFORCED DATE
06/14/24	06/14/24	07/14/24



NOTES:

1. THE SLOPE OF THE DRIVE WHERE SIDEWALKS CROSS SHALL HAVE A MAXIMUM CROSS SLOPE OF 2%.
2. REMOVE ANY EXISTING SIDEWALK AT NEAREST JOINT AND CONNECT REPLACED SECTION TO DRIVE WITH (3) - NO. 4 SMOOTH DOWEL BARS ON 18" CENTERS WITH 3/4" REDWOOD EXPANSION JOINT, WITH 1" REMOVABLE CAP STRIP. SEAL WITH SELF LEVELING GRAY SILICONE SEALANT.
3. COMMERCIAL DRIVE APPROACH 10% MAX SLOPE*;
MIN. 7" SLAB THICKNESS
* MAXIMUM SLOPE AS APPROVED BY OWNER
4. ALL CONNECTIONS TO STATE RIGHT-OF-WAY SHALL USE TXDOT DETAILS.
5. ALL CURB AND GUTTER SHALL BE 30" UNLESS OTHERWISE DIRECTED BY THE CITY.
6. CONCRETE SHALL BE CLASS C, 6 SACK AND HAVE COMPRESSIVE STRENGTH OF 3600 PSI @28 DAYS.
7. IF STREET IS BEING REPLACED, PAVEMENT THICKNESS SHALL BE 6" FOR RESIDENTIAL AND 8" FOR COLLECTOR OR LARGER.

COMMERCIAL DRIVEWAY APPROACH

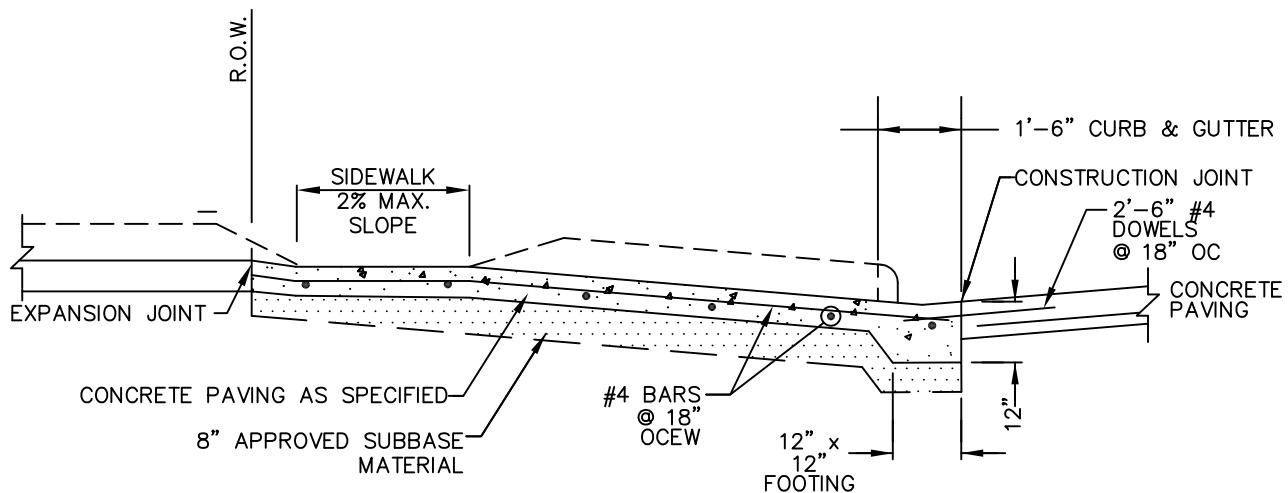
North Central Texas Council of Governments



STANDARD SPECIFICATION REFERENCE

305.2

DATE	STANDARD DRAWING NO.
AUG '23	2155A



SECTION 'A-A'
N.T.S.

COMMERCIAL APPROACH (CONCRETE PAVING)

NOTES:

1. THE SLOPE OF THE DRIVE WHERE SIDEWALKS CROSS SHALL HAVE A MAXIMUM CROSS SLOPE OF 2%.
2. REMOVE ANY EXISTING SIDEWALK AT NEAREST JOINT AND CONNECT REPLACED SECTION TO DRIVE WITH (3) - NO. 4 SMOOTH DOWEL BARS ON 18" CENTERS WITH 3/4" REDWOOD EXPANSION JOINT, WITH 1" REMOVABLE CAP STRIP. SEAL WITH SELF LEVELING GRAY SILICONE SEALANT.
3. COMMERCIAL DRIVE APPROACH 10% MAX SLOPE*; MIN. 7" SLAB THICKNESS
* MAXIMUM SLOPE AS APPROVED BY OWNER
4. ALL CONNECTIONS TO STATE RIGHT-OF-WAY SHALL USE TXDOT DETAILS.
5. ALL CURB AND GUTTER SHALL BE 30" UNLESS OTHERWISE DIRECTED BY THE CITY.
6. CONCRETE SHALL BE CLASS C, 6 SACK AND HAVE COMPRESSIVE STRENGTH OF 3600 PSI @28 DAYS.
7. IF STREET IS BEING REPLACED, PAVEMENT THICKNESS SHALL BE 6" FOR RESIDENTIAL AND 8" FOR COLLECTOR OR LARGER.

COMMERCIAL DRIVEWAY APPROACH
SECTION VIEW

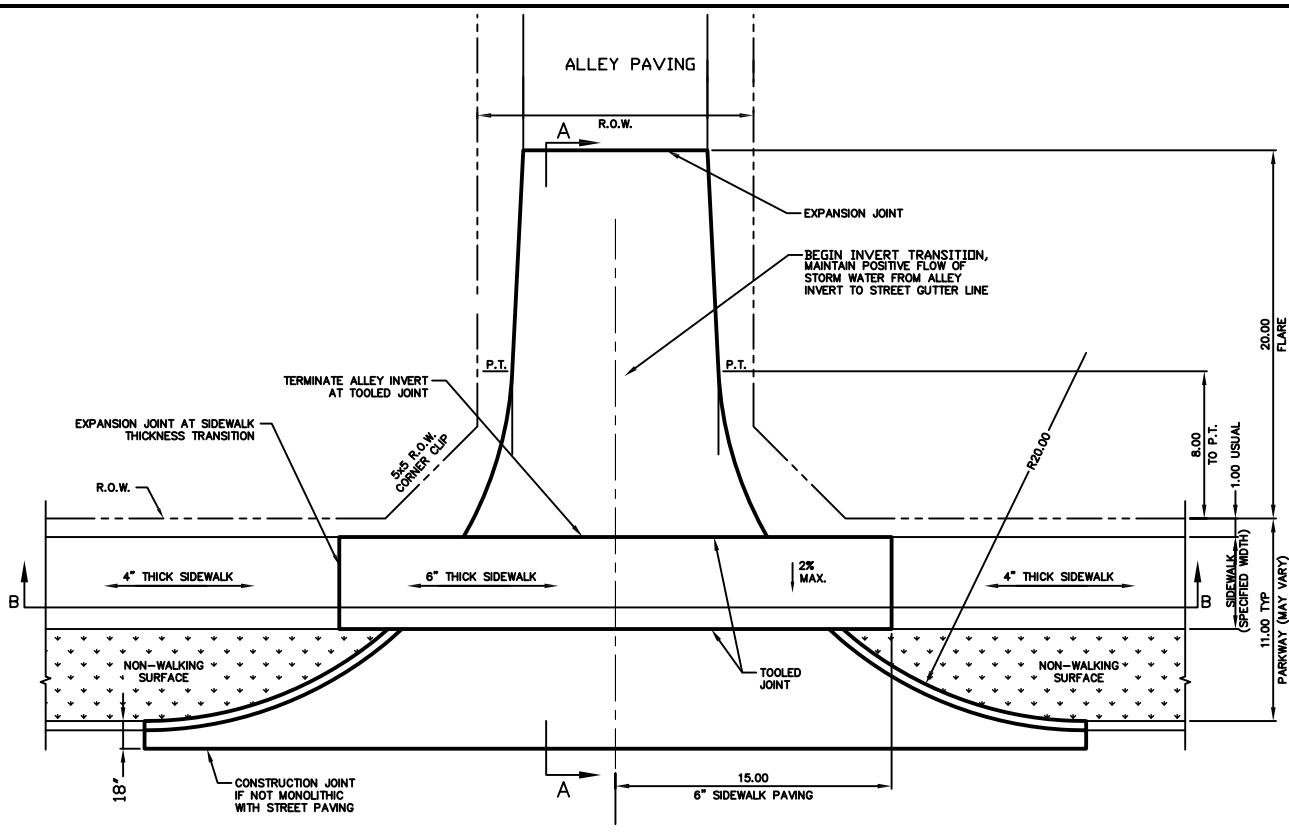
North Central Texas Council of Governments



STANDARD SPECIFICATION REFERENCE

305

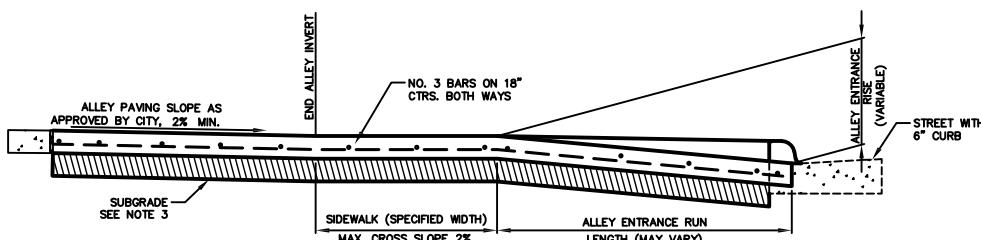
DATE AUG '23	STANDARD DRAWING NO. 2155B
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ALLEY APPROACH RADIUS RETURN TYPE

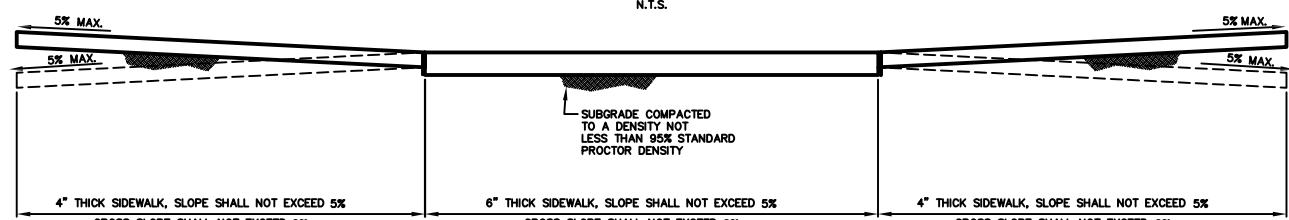
NOTES:

1. THE SLOPE OF THE DRIVE WHERE SIDEWALKS CROSS SHALL HAVE A MAXIMUM CROSS SLOPE OF 2%.
2. REMOVE ANY EXISTING SIDEWALK AT NEAREST JOINT AND CONNECT REPLACED SECTION TO DRIVE WITH (3) - NO. 4 SMOOTH DOWEL BARS ON 18" CENTERS WITH 3/4" REDWOOD EXPANSION JOINT, WITH 1" REMOVABLE CAP STRIP. SEAL WITH SELF LEVELING GRAY SILICONE SEALANT.
3. COMMERCIAL DRIVE APPROACH 10% MAX SLOPE*; MIN. 7" SLAB THICKNESS
* MAXIMUM SLOPE AS APPROVED BY OWNER
4. ALL CONNECTIONS TO STATE RIGHT-OF-WAY SHALL USE TXDOT DETAILS.
5. ALL CURB AND GUTTER SHALL BE 30" UNLESS OTHERWISE DIRECTED BY THE CITY.
6. CONCRETE SHALL BE CLASS C, 6 SACK AND HAVE COMPRESSIVE STRENGTH OF 3600 PSI @28 DAYS.
7. IF STREET IS BEING REPLACED, PAVEMENT THICKNESS SHALL BE 6" FOR RESIDENTIAL AND 8" FOR COLLECTOR OR LARGER.



SECTION A-A

SECTION B-B



NOTES:

1. MIN. PAVEMENT DEPTH AND STRENGTH SHALL BE 6" - CLASS "C", OR AS SPECIFIED BY CITY.
2. CURB HEIGHT AND WIDTH SHALL BE 6", OR AS SPECIFIED BY CITY. SEE STANDARD CONSTRUCTION DETAIL 2120.
3. SUBGRADE SHALL MATCH ALLEY PAVEMENT SUBGRADE

ALLEY APPROACH
RADIUS RETURN TYPE

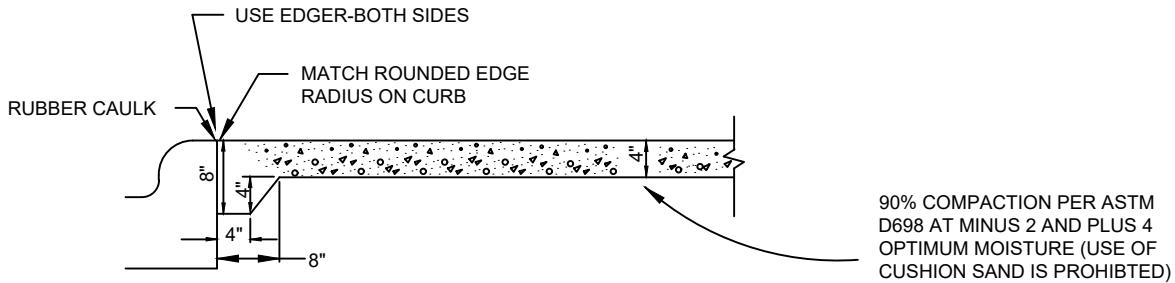
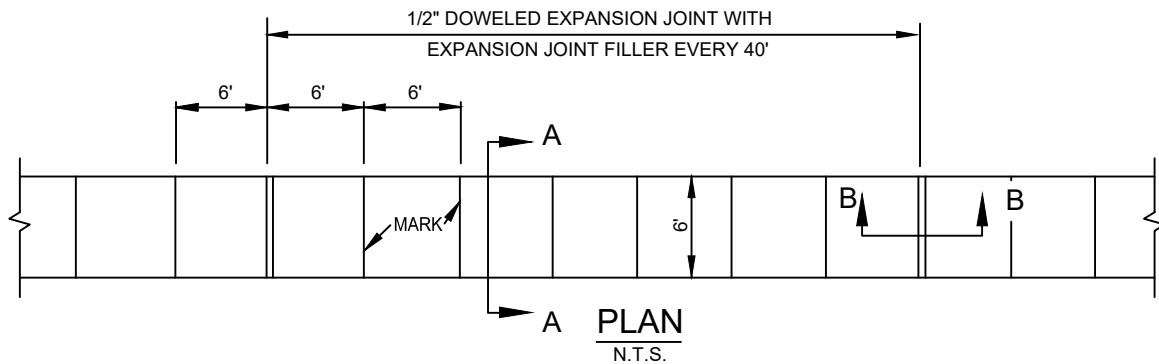


North Central Texas Council of Governments

STANDARD SPECIFICATION REFERENCE

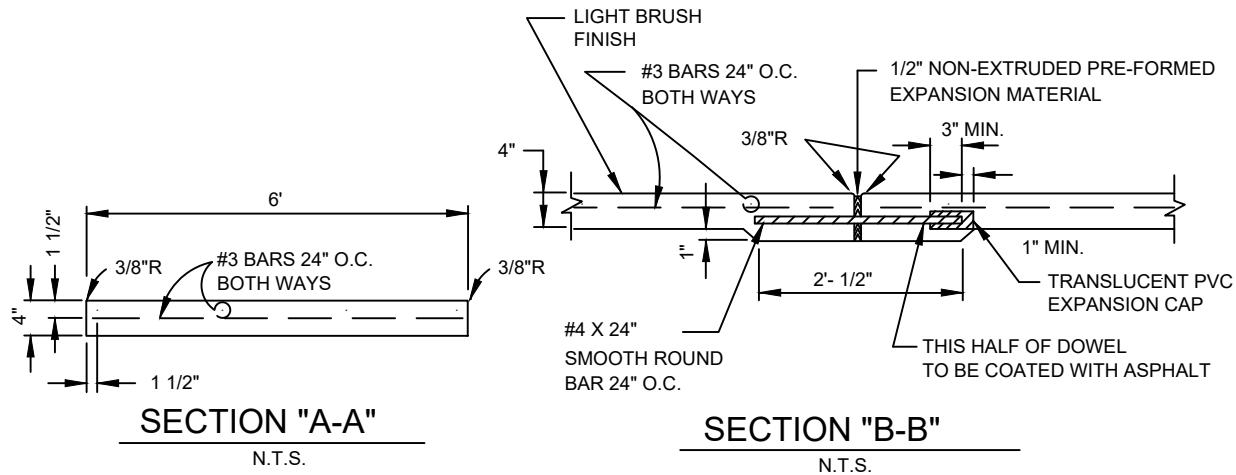
305.2

DATE AUG '23 STANDARD DRAWING NO. 2160



JOINT LUG DETAIL FOR MEDIAN PAVEMENT OR SIDEWALK ADJACENT TO CURB

N.T.S.



NOTE:

1. REFER TO NCTCOG SECTION 305.2 FOR ALL SIDEWALK SPECIFICATIONS.
2. CROSS SLOPE OF SIDEWALK SHALL BE $\pm 1/4"$ PER FT. MIN. TO $\pm 3/8"$ PER FT. MAX.
3. 5'-0" SIDEWALK FOR RESIDENTIAL STREETS AND 6'-0" SIDEWALK FOR COLLECTOR AND THOROUGHFARE STREETS.
4. SIDEWALK SHALL BE CLASS "A" CONCRETE UNLESS OTHERWISE SPECIFIED BY OWNER.
5. ALL HONEYCOMB IN BACK OF CURB TO BE TROWEL-PLASTERED BEFORE POURING SIDEWALK.
6. LUG MAY BE FORMED BY SHAPING SUBGRADE TO APPROXIMATE DIMENSIONS SHOWN.

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REINFORCED CONCRETE SIDEWALKS		NCTCOG STANDARD SPECIFICATION REFERENCE	
JOINTS AND SPACING		305.2	
NOTICE DATE	MODIFIED DATE	STANDARD DRAWING NO.	ENFORCEMENT DATE
07/28/23	07/28/23	2170M*	08/28/23

This detail and accompanying notes are a representation of the information from PROWAG and are intended to provide guidance for the design and construction of sidewalk ramps in Melissa. It is the responsibility of the Design Professional and the Contractor to ensure that all ramps constructed meet the requirements of PROWAG.

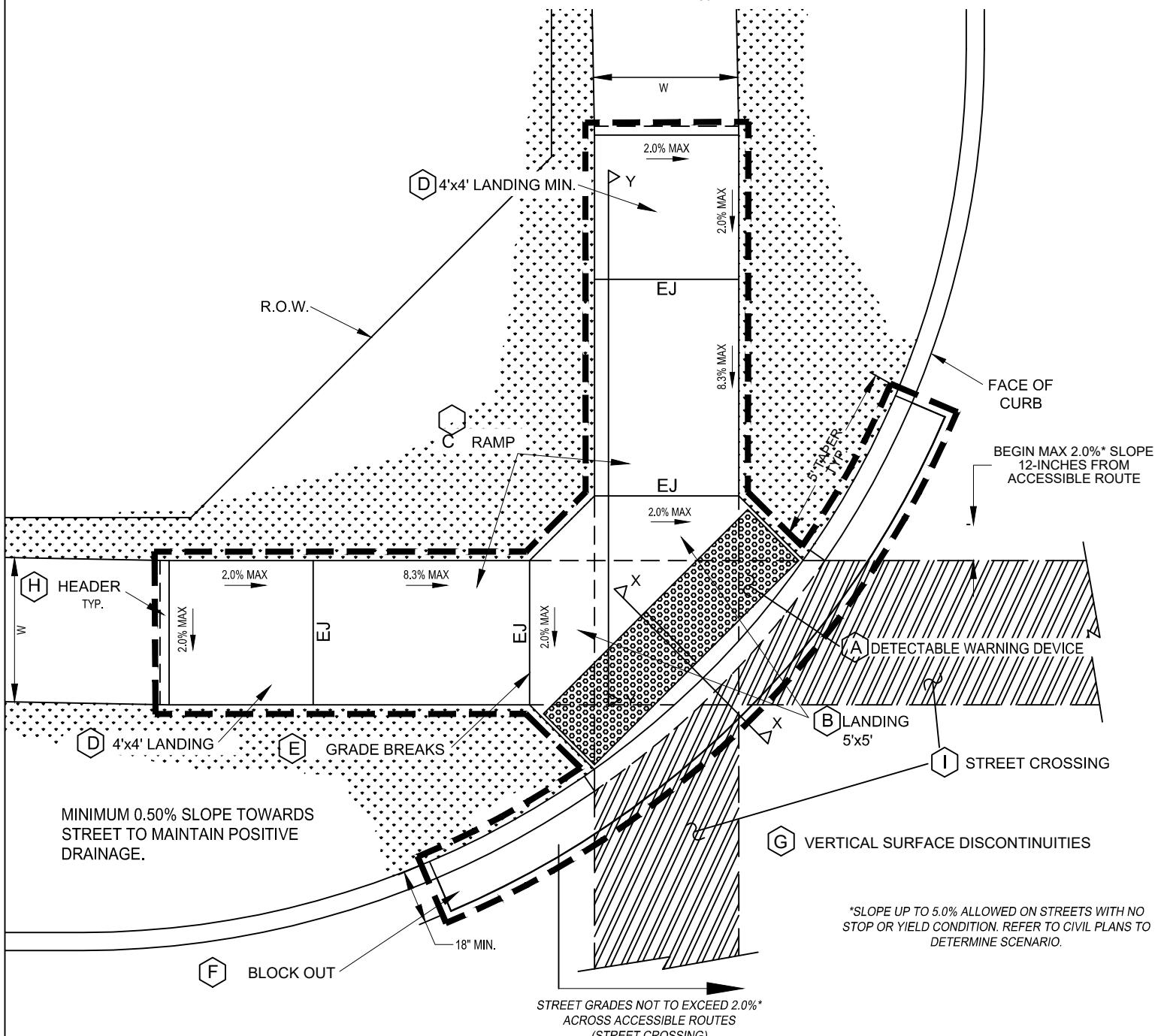
EJ - Expansion Joint

CJ - Construction Joint

— — — — — Required limits of construction with street installation

PROWAG = Public Rights-of-Way Accessibility Guidelines

W - Sidewalk Width



NOTES: All newly constructed sidewalks, curb ramps and crosswalks installed within City of Melissa public rights-of-way shall be considered a pedestrian access route and shall conform to the most current "Draft" Guidelines for Public Rights-of-Way created by the United States Access Board.

1. See Detail 2171BM for ramp feature descriptions.
2. See Detail 2171CM for Sections X-X and Y-Y.
3. See Detail 2171DM for Additional Notes.

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 MELISSA TX	NCTCOG STANDARD SPECIFICATION REFERENCE	
	305.2	
MODIFIED DATE	STANDARD DRAWING NO.	
07/28/23	2171AM*	
NOTICE DATE	APPLIED DATE	ENFORCED DATE
07/28/23	07/28/23	08/28/23

PEDESTRIAN FACILITIES

CURB RAMPS

- A** Detectable Warning Devices (DWD) shall be pre-manufactured cast-in-place plates or brick pavers approved by the City of Melissa, installed to the manufacturer's specifications, and meet all ADA requirements. Color to be Brick Red or similar. DWD shall be 24 inches in length for the full width of the street connection starting at the back of curb. A maximum 2-inch border shall be allowed on the sides of the DWD for proper installation.
- B** Also known as "Clear Space" per ADA PROWAG, the City requires a minimum landing space of 4-foot by 4-foot at the bottom of every ramp. This landing space shall have a cross slope in both directions that does not exceed 2.0%. and shall be wholly outside the parallel vehicular travel path.
- C** The ramp component of the directional curb ramp shall have a continuous longitudinal slope more than 5% and less than 8.3%. The ramp shall also have a cross slope of no more than 2.0%. Length of ramp can vary, but shall not exceed 15 feet to achieve desired elevation change.
- D** Also known as "Turning Space" per ADA PROWAG, a minimum landing space of 4-foot by 4-foot shall be at the top of every ramp. This landing (turning) space shall have a cross slope in both directions that does not exceed 2.0%. Landing must match width of sidewalk and length shall be the same distance ("Squared" Landing).
- E** All curb ramps shall have grade breaks at the top and bottom that are perpendicular to the direction of the ramp run. Where the ends of the bottom grade break are less than or equal to 5 feet, the DWD shall be placed within the ramp at the bottom grade break. Where either end of the bottom grade break is greater than 5 feet, the DWD shall be placed behind the back of the curb.
- F** Paving contractor shall leave block out with a keyway joint installed, minimum of 18 inches measured from back of curb. Block out shall be poured monolithically with Curb Ramp. Concrete shall tie to street paving with a keyway joint per City standards. No curb shall be constructed where a DWD is provided. The curb on either side shall have a typical 5 foot taper to transition from the standard 6-inch curb height to be flush with ramp.
- G** All work associated with accessible routes shall be installed flush with all features to minimize vertical surface discontinuities. Each segment along accessible route shall be flush with no more (zero tolerance) than a $\frac{1}{4}$ -inch grade separation (elevation difference), or $\frac{1}{2}$ -inch grade separation if beveled (bevel slope shall not be steeper than 50%).
- H** A sidewalk header shall be constructed at ends of all work performed.
- I** Street crossings shall adhere to same guidelines as other accessible routes within public right-of-way, and shall be for the full width of the in-line accessible route. Cross slope shall not exceed 2%*. New street construction shall incorporate all ADA design requirements. It shall be the responsibility of the Design Professional and Contractor to ensure all street crossings meet the requirements of PROWAG. Street alterations on existing streets to bring to compliance shall be at the City Engineer's discretion.
- J** All curbs constructed as part of an ADA Ramp shall match City curb standards.

* See PROWAG special design considerations when street crossing has no stop or yield condition.

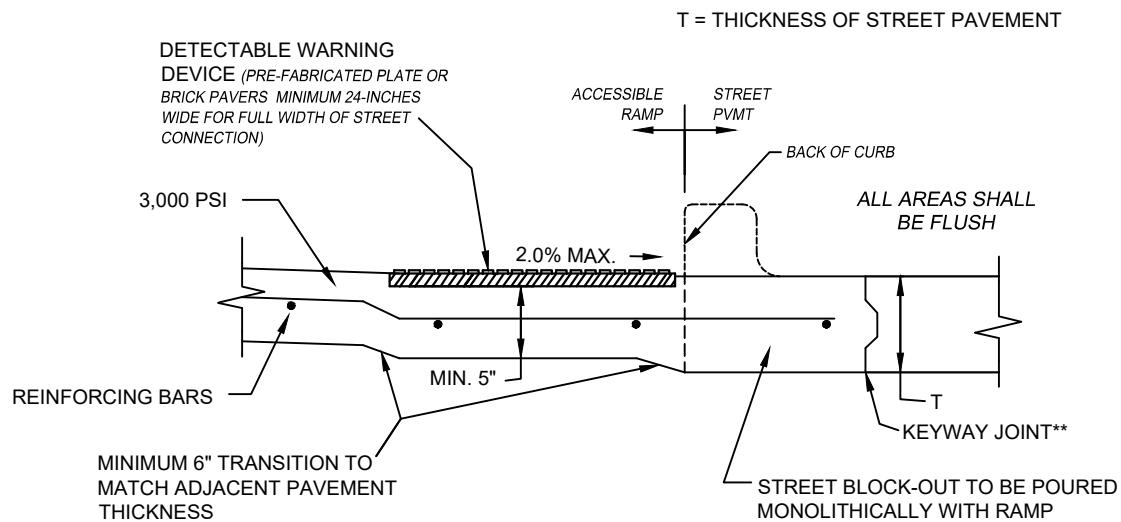
M* - CITY OF MELISSA REVISION

 MELISSA TX	NCTCOG STANDARD SPECIFICATION REFERENCE	
	305.2	
	MODIFIED DATE	STANDARD DRAWING NO.
	07/28/23	2171BM*

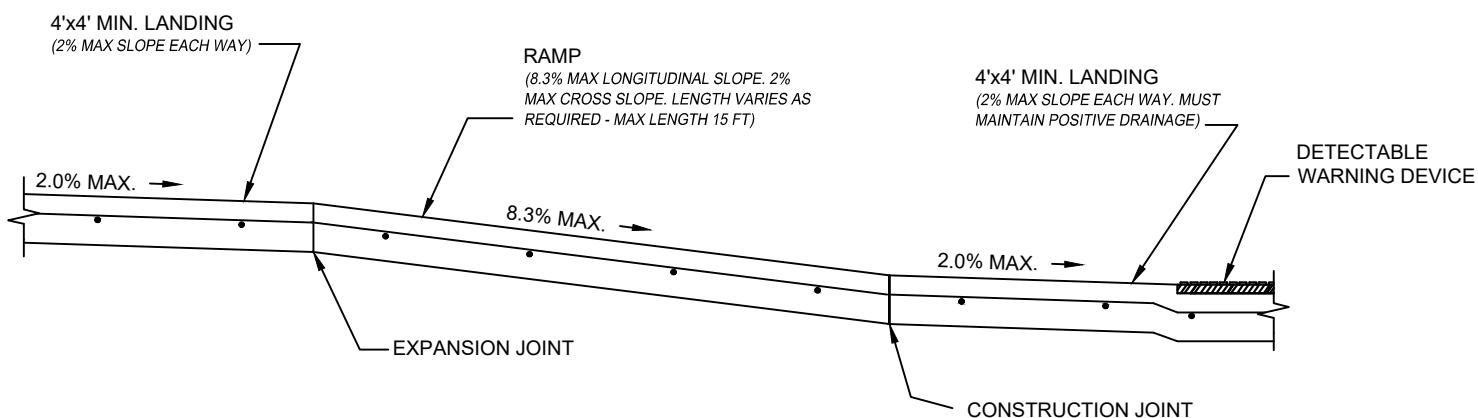
PEDESTRIAN FACILITIES

CURB RAMPS

NOTICE DATE	APPLIED DATE	ENFORCED DATE
07/28/23	07/28/23	08/28/23



**KEYWAY JOINT FOR NEW CONSTRUCTION. STREET CONNECTION SHALL BE LONGITUDINAL BUTT JOINT FOR CONNECTIONS TO EXISTING ROADWAYS.



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 MELISSA TX	NCTCOG STANDARD SPECIFICATION REFERENCE	
	305.2	
MODIFIED DATE	STANDARD DRAWING NO.	
07/28/23	2171CM*	
NOTICE DATE	APPLIED DATE	ENFORCED DATE
07/28/23	07/28/23	08/28/23

PEDESTRIAN FACILITIES
CURB RAMPS

PEDESTRIAN ACCESSIBILITY (WITHIN PUBLIC R.O.W.)

All newly constructed sidewalks, curb ramps and crosswalks installed within City of Melissa public rights-of-way shall be considered a pedestrian access route and shall conform to the most current "Draft" Guidelines for Public Rights-of-Way created by the United States Access Board.

CURB RAMPS

1. All slopes shown are **MAXIMUM ALLOWABLE**. Lesser slopes that will still drain properly should be used. Adjust curb ramp length or grade of approach sidewalks as directed.
2. Landings shall be 4'x 4' minimum with a maximum 2% slope in the transverse and longitudinal directions..
3. Clear space at the bottom of curb ramps shall be a minimum of 4'x 4' wholly contained within the crosswalk and wholly outside the parallel vehicular travel path.
4. Maximum allowable cross slope on sidewalk and curb ramp surfaces is 2%.
5. Additional information on curb ramp location, design, light reflective value and texture may be found in the most current edition of the Texas Accessibility Standards (TAS) and 16 TAC 68.102. Federal guidelines shall supersede any conflicts.
6. Crosswalk dimensions, crosswalk markings and stop bar locations shall be as shown elsewhere in the plans. At intersections where crosswalk markings are not required, curb ramps and accessible routes shall align with theoretical crosswalks unless otherwise directed.
7. Handrails are not required on curb ramps.
8. Provide a flush transition where the curb ramps connect to the street.
9. Accessible routes are considered "ramps" when longitudinal slopes are between 5% and 8.3% (maximum allowable). Sidewalks under 5% longitudinal slope are deemed accessible routes and must follow all applicable guidelines.

DETECTABLE WARNING DEVICE

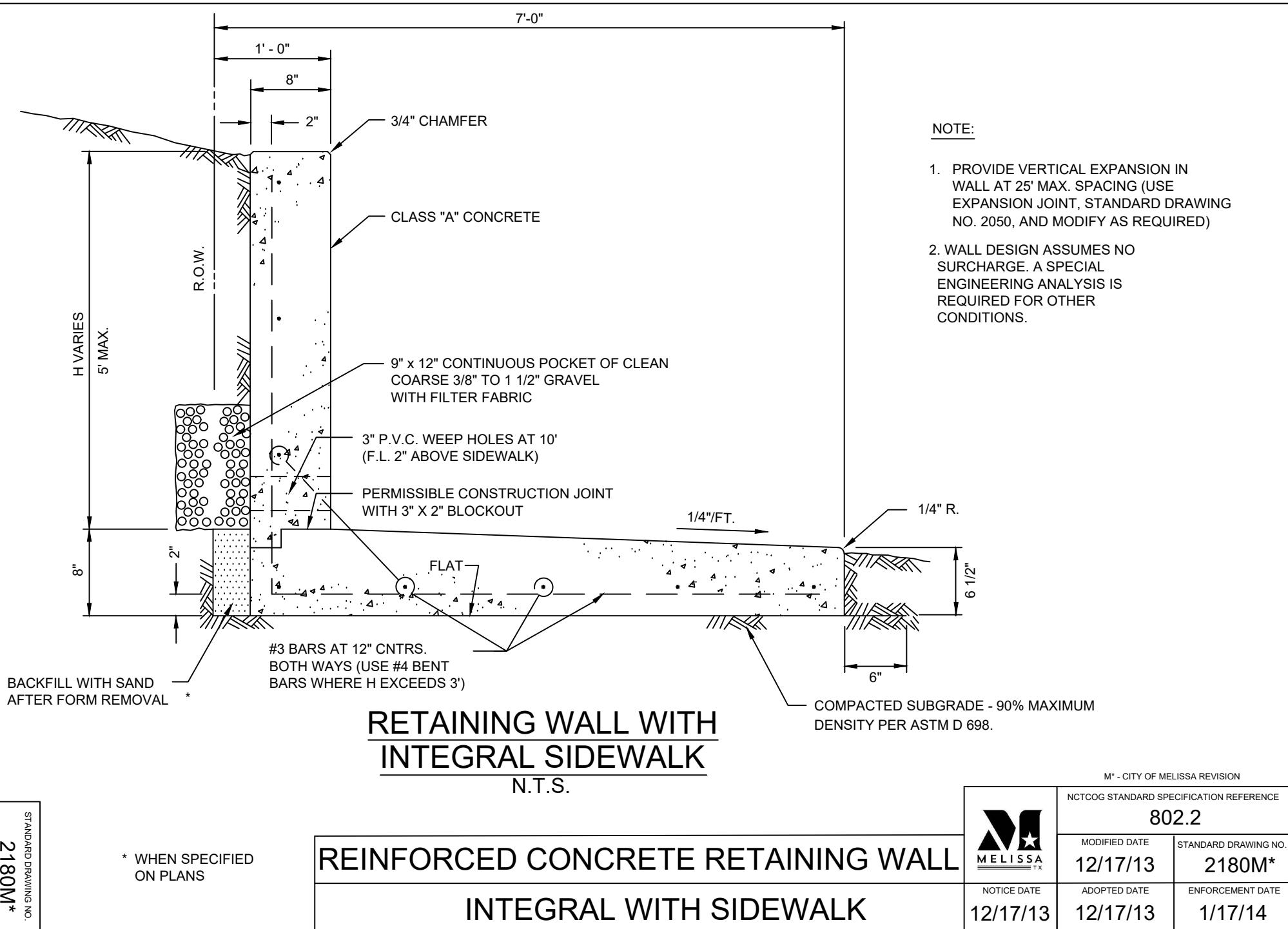
10. Curb ramps must contain a detectable warning surface that consists of raised truncated domes complying with Section 705 of the TAS. The surface must contrast visually with adjoining surfaces. Furnish and install an approved cast-in-place dark red detectable warning surface material adjacent to uncolored concrete, unless specified elsewhere in the plans.
11. Detectable Warning Materials must meet City of Melissa Material Specifications (Refer to TxDOT Approved Vendor List) and be listed on the Material Producer List. Install products in accordance with manufacturer's specifications.
12. Detectable warning surfaces must be slip resistant and not allow water to accumulate.
13. Detectable warning surfaces shall be a minimum of 24" in depth in the direction of pedestrian travel, and extend the full width of the curb ramp or landing where the pedestrian access route enters the street.
14. Detectable warning surfaces shall be located so that the edge nearest the curb line is at the back of curb. When placed on the ramp, align the rows of domes to be perpendicular to the grade break between the ramp run and the street. Where detectable warning surfaces are provided on a surface with a slope that is less than 5 percent, dome orientation is less critical. Detectable warning surfaces may be curved along the corner radius.

SIDEWALKS

15. Provide clear ground space at operable parts, including pedestrian push buttons. Operable parts shall be placed within one or more reach ranges specified in TAS 308.
16. Place traffic signal or illumination poles, ground boxes, controller boxes, signs, drainage facilities and other items so as not to obstruct the pedestrian access route or clear ground space.
17. Street grades and cross slopes shall be as shown elsewhere in the plans.
18. Changes in level greater than 1/4 inch are not permitted (1/2 inch with bevel).
19. Where a 4' sidewalk is provided, a 5'x 5' passing areas are required at intervals not to exceed 200'.
20. The least possible grade should be used to maximize accessibility. The running slope of sidewalks and crosswalks within the public right of way may follow the grade of the parallel roadway. Where a continuous grade greater than 5% must be provided, handrails may be desirable to improve accessibility. Handrails may also be needed to protect pedestrians from potentially hazardous conditions. If provided, handrails shall comply with TAS 505.
21. Handrail extensions shall not protrude into the usable landing area or into intersecting pedestrian routes.

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 MELISSA TX	305.2	
	MODIFIED DATE 07/28/23	STANDARD DRAWING NO. 2171DM*
PEDESTRIAN FACILITIES	NOTICE DATE 07/28/23	APPLIED DATE 07/28/23
CURB RAMPS	ENFORCED DATE 08/28/23	



GENERAL NOTES:

1. REINFORCED CONCRETE PAVEMENT:
 - A. ALL CURBS SHALL BE PLACED INTEGRAL WITH PAVEMENT UNLESS OTHERWISE APPROVED BY THE OWNER.
 - B. CURBS SHALL MEET THE SAME COMPRESSIVE STRENGTH AS SPECIFIED FOR THE PAVEMENT.
 - C. CONTRACTOR SHALL PROVIDE STRAIGHT DEFORMED STEEL TIE BARS IN ACCORDANCE WITH THE TXDOT STANDARD SPECIFICATIONS FOR CONSTRUCTION AND MAINTENANCE OF HIGHWAYS, STREETS, AND BRIDGES ITEM 360.2.2.2.
 - D. REINFORCING BARS SHALL BE SUPPORTED BY CHAIRS OR OTHER DEVICES APPROVED BY THE OWNER.
 - E. RESIDENTIAL STREETS SHALL BE MACHINE PLACED BY A SLIP FORM PAVER UNLESS OTHERWISE APPROVED BY THE CITY, HAVE A MINIMUM PAVEMENT THICKNESS OF 6" AND A MINIMUM 3600 PSI COMPRESSIVE STRENGTH.
 - F. THOROUGHFARES SHALL BE MACHINE PLACED BY A SLIP FORM PAVER UNLESS OTHERWISE APPROVED BY THE CITY, HAVE A MINIMUM PAVEMENT THICKNESS OF 9" AND A MINIMUM 3600 PSI COMPRESSIVE STRENGTH.
 - G. HAND PLACED CONCRETE SHALL HAVE A MINIMUM 4500 PSI COMPRESSIVE STRENGTH.
2. SUBGRADE: (UNLESS OTHERWISE SPECIFIED BY OWNER)
 - A. SUBGRADE UNDER ALL PAVEMENTS SHALL BE STABILIZED TO A MINIMUM DEPTH OF 6" WITH HYDRATED LIME OR CEMENT WHEN THE P.I. OF THE INPLACE MATERIAL IS GREATER THAN 15. LABORATORY TESTS MUST BE PERFORMED TO DETERMINE THE AMOUNT OF LIME OR CEMENT REQUIRED TO LOWER THE P.I. TO 15 OR BELOW. SATURATION P.I. (PH > 12.4) WILL BE THE LIMIT WHEN A SOIL'S P.I. CANNOT BE BROUGHT TO 15 OR LOWER.
 - B. WHERE THE INPLACE MATERIAL HAS A P.I. OF LESS THAN 15, THE SUBGRADE SHALL BE SCARIFIED TO A MINIMUM DEPTH OF 6" AND RECOMPACTED.
 - C. THE SUBGRADE SHALL BE PROOF ROLLED PER THE NCTCOG PRIOR TO PAVING TO LOCATE ANY UNSTABLE AREAS OF EARTHWORK OR BASE.
 - D. DENSITY TESTING SHALL BE PERFORMED PER THE TXDOT STANDARD SPECIFICATIONS FOR CONSTRUCTION AND MAINTENANCE OF HIGHWAYS, STREETS, AND BRIDGES ITEM 132.
3. IF THE ROADWAY IS A DESIGNATED BIKE ROUTE OR BIKE USAGE IS ANTICIPATED, REFER TO NCTCOG'S REGIONAL BICYCLE AND PEDESTRIAN FACILITIES DESIGN MANUAL FOR DESIGN GUIDANCE.

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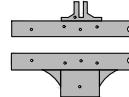
NCTCOG STANDARD SPECIFICATION REFERENCE	
302, 303	
MODIFIED DATE	STANDARD DRAWING NO.
07/28/23	2190M
NOTICE DATE	ADOPTED DATE
07/28/23	07/28/23
ENFORCEMENT DATE	
	08/28/23

PAVEMENT SYSTEMS

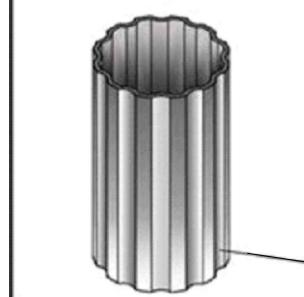
GENERAL NOTES



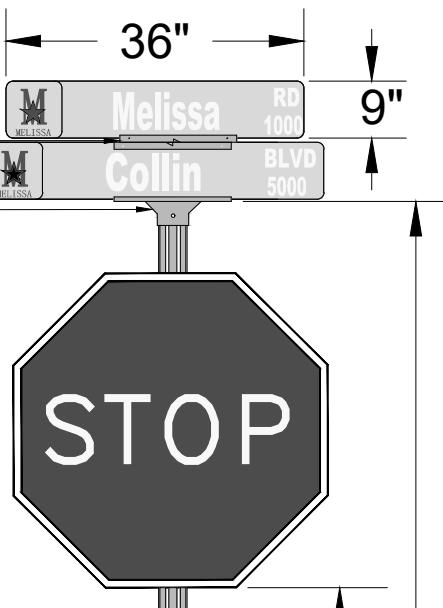
3X2WAY12 & 4WAY12 BRACKETS



SP3X(POLE LENGTH)
3" DIAMETER FLUTED SIGN POLE
.125" WALL THICKNESS



SLIP OVER POLE AND
TIGHTEN THE SET SCREWS
IN THE TOP OF BASE



10'

CITY OF MELISSA STREET SIGNAGE DETAIL

SUPER ENGINEER GRADE (SEG) TYPE II REFLECTIVE WHITE SHEETING WITH BLACK ESM OVERLAY ON 0.080" ALUMINUM FLAT BLADE. ALL SIGNAGE TO BE RETRO-REFLECTIVE. HIGHWAY SERIES "B" FRONT SHALL BE USED FOR THE STREET ID'S.

STREET NAMES:

SHALL BE LETTER UPPER CASE FOR EACH WORD CAPITALIZED AND LOWER CASE THEREAFTER. NAME SHALL BE 6" TALL FOR UPPERCASE LETTERS AND 4.5" TALL FOR LOWERCASE.

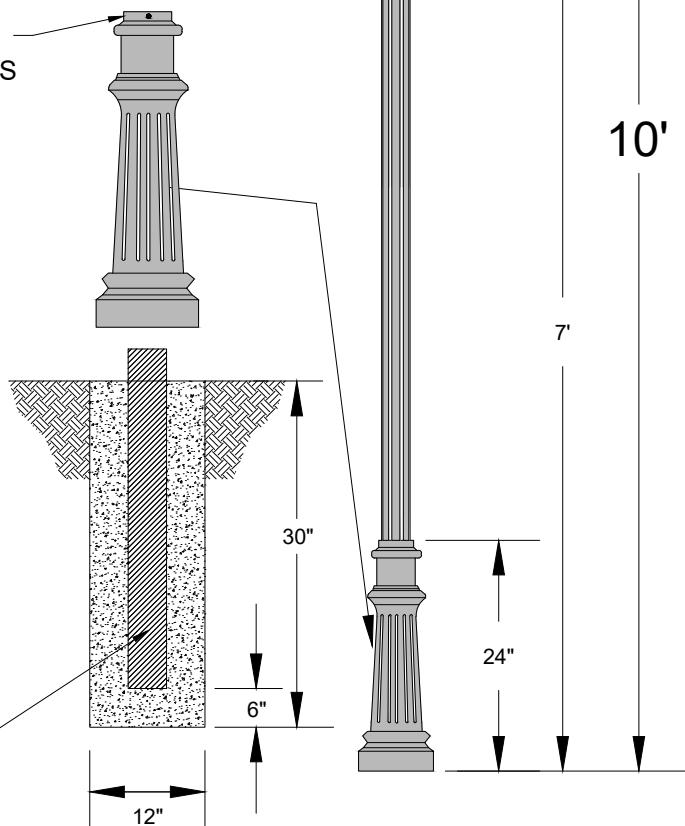
CITY LOGO:

SHALL BE APPROVED CITY LOGO AS SHOWN AND 6" IN HEIGHT.

ROAD DESIGNATION AND BLOCK NUMBERING:

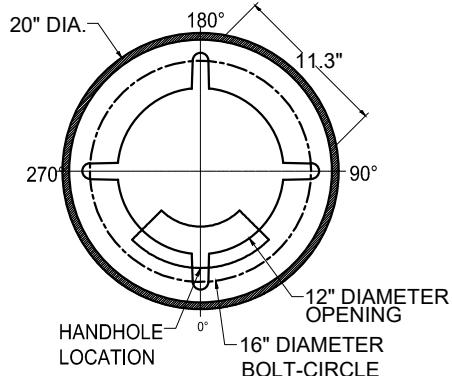
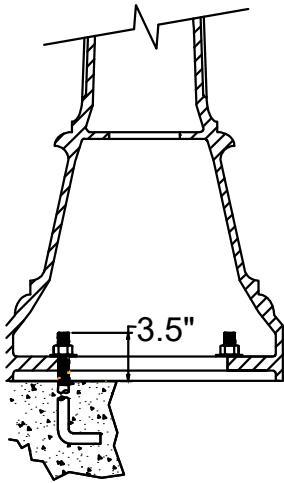
ROAD DESIGNATION SHALL APPEAR IN THE UPPER RIGHT CORNER AND BLOCK NUMBER INFORMATION IN THE LOWER RIGHT CORNER.

12' LONG, 3" DIAMETER
BLACK ALUMINUM POST
WITH 2' EMBEDDED IN
3,000 PSI CONCRETE & END
PLUGGED TO PREVENT
CONCRETE INFILTRATION.



M* - CITY OF MELISSA REVISION

RESIDENTIAL STREET SIGNAGE		NCTCOG STANDARD SPECIFICATION REFERENCE	
CITY OF MELISSA		MODIFIED DATE	STANDARD DRAWING NO.
NOTICE DATE	07/28/23	ADOPTED DATE	07/28/23
ENFORCEMENT DATE	08/28/23		



ANCHORAGE DETAIL

NO SCALE



MELISSA



SPECIFICATIONS:

GENERAL DESCRIPTION:

THE LIGHTING POST SHALL BE ALL ALUMINUM, ONE-PIECE CONSTRUCTION, WITH A CLASSIC TAPERED AND FLUTED BASE DESIGN.

MATERIALS:

THE BASE SHALL BE HEAVY WALL, CAST ALUMINUM PRODUCED FROM CERTIFIED ASTM 356.1 INGOT PER ASTM-B-179-95a OR ASTM B26-95. THE STRAIGHT SHAFTS SHALL BE EXTRUDED FROM ALUMINUM, ASTM 6061 ALLOY, HEAT TREATED TO A T6 TEMPER. THE TAPERED SHAFT SHALL BE EXTRUDED FROM ALUMINUM, ASTM 6063 ALLOY, SPUN TO A TAPERED SHAPE, THEN HEAT TREATED TO A T6 TEMPER. ALL HARDWARE SHALL BE TAPER RESISTANT STAINLESS STEEL. ANCHOR BOLTS TO BE COMPLETELY HOT DIP GALVANIZED.

CONSTRUCTION:

THE SHAFT SHALL BE DOUBLE WELDED TO THE BASE CASTING AND SHIPPED AS ONE PIECE FOR MAXIMUM STRUCTURAL INTEGRITY. THE SHAFT SHALL BE WELDED INSIDE THE BASE CASTING AT THE TOP OF THE ACCESS DOOR, AND EXTERNALLY WHERE THE SHAFT EXITS THE BASE. ALL WELDING SHALL BE PER ANSI/AWS.

DIMENSIONS:

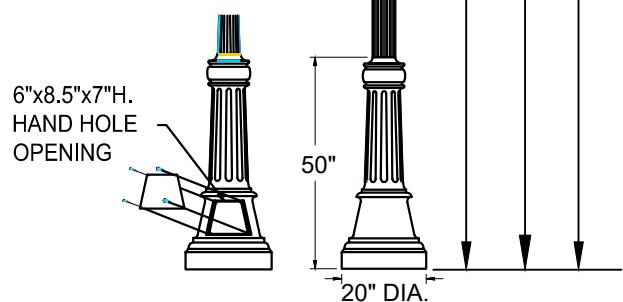
THE POST SHALL BE 12'-0" TO 14'-0" IN HEIGHT WITH A DIAMETER BASE. THE SHAFT DIAMETER SHALL BE 4". AT THE TOP OF THE POST, AN INTEGRAL TENON WITH A TRANSITIONAL DONUT SHALL BE PROVIDED FOR LUMINAIRE MOUNTING.

INSTALLATION:

THE POST SHALL BE PROVIDED WITH FOUR, HOT DIP GALVANIZED L-TYPE ANCHOR BOLTS. A DOOR SHALL BE PROVIDED IN THE BASE FOR ANCHORAGE AND WIRING ACCESS. A GROUNDING SCREW SHALL BE PROVIDED INSIDE THE BASE OPPOSITE THE DOOR.

LAMP BASE DETAIL

NO SCALE



NOTE:

SEE 2300CM & 2300DM FOR LUMINAIRE, FOUNDATION AND ELECTRICAL DETAILS

RESIDENTIAL LIGHT POST STREET SIGNAGE

CITY OF MELISSA

M* - CITY OF MELISSA REVISION

NCTCOG STANDARD SPECIFICATION REFERENCE

MODIFIED DATE	STANDARD DRAWING NO.
08/08/13	2300BM*
NOTICE DATE	ADOPTED DATE
08/08/13	08/08/13

08/08/13

08/08/13

09/08/13

SPECIFICATIONS:**GENERAL DESCRIPTION:**

THE LED SHALL BE DESIGNED FOR EASE OF MAINTENANCE WITH THE PLUG-IN ELECTRICAL MODULE. A PRECISION OPTICAL SYSTEM MAXIMIZES POST SPACING WHILE MAINTAINING UNIFORM ILLUMINATION.

OPTICAL SYSTEM:

THE OPTICAL SYSTEM SHALL CONSIST OF A PRECISELY ENGINEERED LED CIRCUIT LOCATED IN THE TOP COVER. A GASKET BETWEEN THE COVER AND RING ALONG WITH A FLAT GLASS PLATE AND GASKET BEHIND THE REFLECTOR CREATE A SEALED OPTICAL COMPARTMENT THAT SHALL MEET AN IP RATING.

LUMINAIR HOUSING:

THE LUMINAIRE HOUSING, CAST OF ALUMINUM, SHALL ANCHOR THE OPTICAL SYSTEM AND PROVIDE AN ENCLOSURE FOR THE PLUG IN ELECTRICAL MODULE. THE ELECTRICAL ENCLOSURE SHALL CONFORM TO AN IP55 RATING. THE NICKEL PLATED LAMP GRIP SOCKET AND THE THREE STATION INCOMING LINE TERMINAL BLOCK SHALL BE PRE-WIRED TO A FIVE CONDUCTOR RECEPTACLE FOR EASE IN CONNECTING THE ELECTRICAL MODULE. A SLIPFITTER WILL ACCEPT A 3" HIGH BY 2 7/8" TO 3 1/8" O.D. POLE TENON.

LUMINAIRE HOUSING DOOR:

CAST OF ALUMINUM, THE HOUSING DOOR SHALL BE HINGED AND LATCHED TO THE HOUSING. THE DOOR FORMS THE MOUNT FOR THE ELECTRICAL MODULE, AND ALLOWS EASY ACCESS DURING MAINTENANCE.

ELECTRICAL MODULE:

THE ELECTRICAL COMPONENTS SHALL BE MOUNTED TO AN ALUMINUM PLATE THAT IS REMOVABLE WITH STANDARD TOOLS. A MATCHING FIVE PLUG CONNECTOR CONNECTS THE ELECTRICAL COMPONENTS TO THE SURGE PROTECTOR FOR QUICK REPLACEMENT.

TOP COVER:

THE TOP COVER, CAST OF ALUMINUM, SHALL BE ATTACHED TO THE TOP RING OF THE LUMINAIRE HOUSING BY A STAINLESS STEEL HINGE PIN. THE COVER SHALL BE LATCHED BY AN OVER CENTER, POSITIVE ACTION, STAINLESS STEEL LATCH WHICH ALLOWS TOOL-LESS ENTRY TO THE LAMP CHAMBER FOR RELAMPING.

DRIVER:

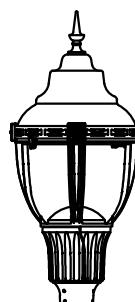
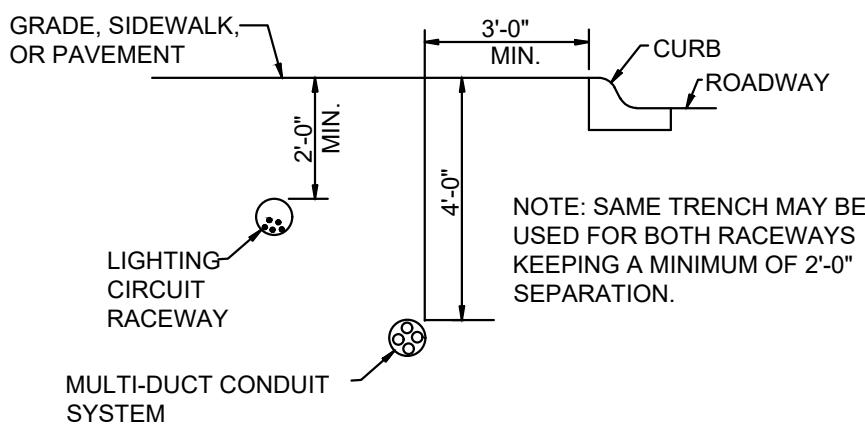
THE DRIVE SHALL BE 50 OR 60 HZ VOLTAGE SENSING 120-277V>90% EFFICIENT ELECTRONIC. POTTED FOR INCREASED THERMAL MANAGEMENT. REMOVABLE THROUGH HINGED DOOR HOUSING.

FINISH:

THE LUMINAIRE SHALL BE FINISHED WITH BLACK POLYESTER POWDER PAINT APPLIED TO ENSURE MAXIMUM DURABILITY.

UL:

THE LUMINAIRE SHALL BE UL LISTED AS SUITABLE FOR WET LOCATIONS AT A MAXIMUM OF 40 DEGREES C AMBIENT TEMPERATURE.

**LUMINAIRE DETAIL**

NO SCALE

TRENCH DETAIL

NO SCALE

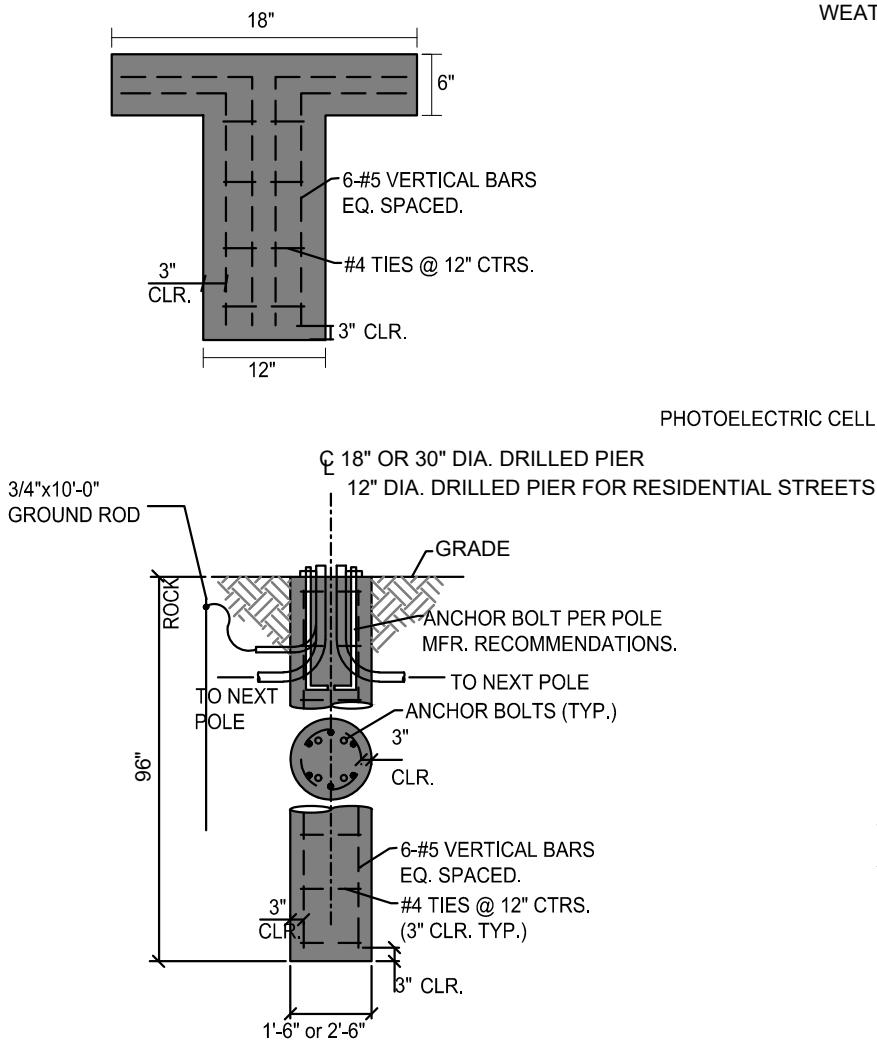
RESIDENTIAL LIGHT POST STREET SIGNAGE**CITY OF MELISSA**

M* - CITY OF MELISSA REVISION		NCTCOG STANDARD SPECIFICATION REFERENCE	
MODIFIED DATE	STANDARD DRAWING NO.	NOTICE DATE	ADOPTED DATE
08/12/13	2300CM*	08/12/13	08/12/13
			09/12/13

KEYED NOTES:

① CIRCUIT BREAKER IN NEMA 3R ENCLOSURE MOUNTED ON SERVICE POLE WITH LIGHTING ARRESTORS AND SURGE CAPACITORS. THE CIRCUIT BREAKER SHALL BE LABELED FOR SERVICE ENTRANCE USE.

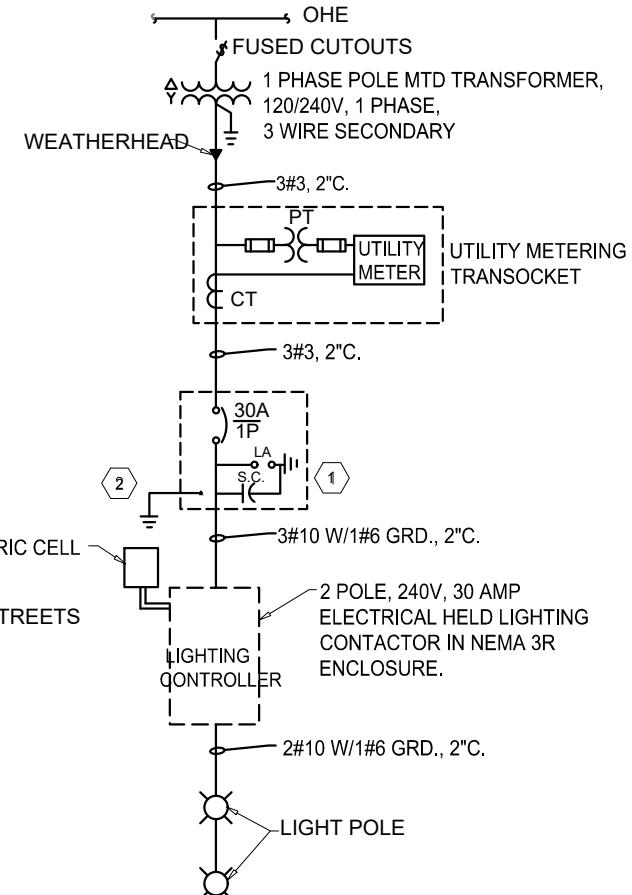
② BOND NEUTRAL CONDUCTOR TO GROUND, WITH A #2/0 BARE BRAIDED CU GROUNDING CONDUCTOR VIA 1" SCH 40 PVC CONDUIT TO 12" BELOW FINISHED GRADE AT THE SERVICE ENTRANCE CIRCUIT BREAKER.

**LIGHT POLE FOUNDATION DETAIL
FOR RESIDENTIAL STREETS**

NOTES:

1. CONCRETE COMP. STRENGTH - 4000 P.S.I.
2. REINFORCEMENT STEEL - ASTM A615-GR 60.
3. CLEAN BOTTOM OF HOLE PRIOR TO PLACEMENT OF CONCRETE.
4. CONCRETE SHALL BE PLACED WITHIN 8 HOURS OF DRILLING.
5. 12" DIA. DRILLED PIERS WILL ONLY BE ALLOWED FOR RESIDENTIAL STREETS

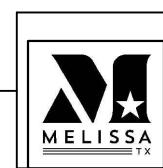
LIGHT POLE FOUNDATION DETAIL

NO SCALE

RESIDENTIAL LIGHT POST STREET SIGNAGE
CITY OF MELISSA

**LIGHTING CONTROLLER
ONE-LINE DIAGRAM**

NO SCALE

M* - CITY OF MELISSA REVISION



NCTCOG STANDARD SPECIFICATION REFERENCE

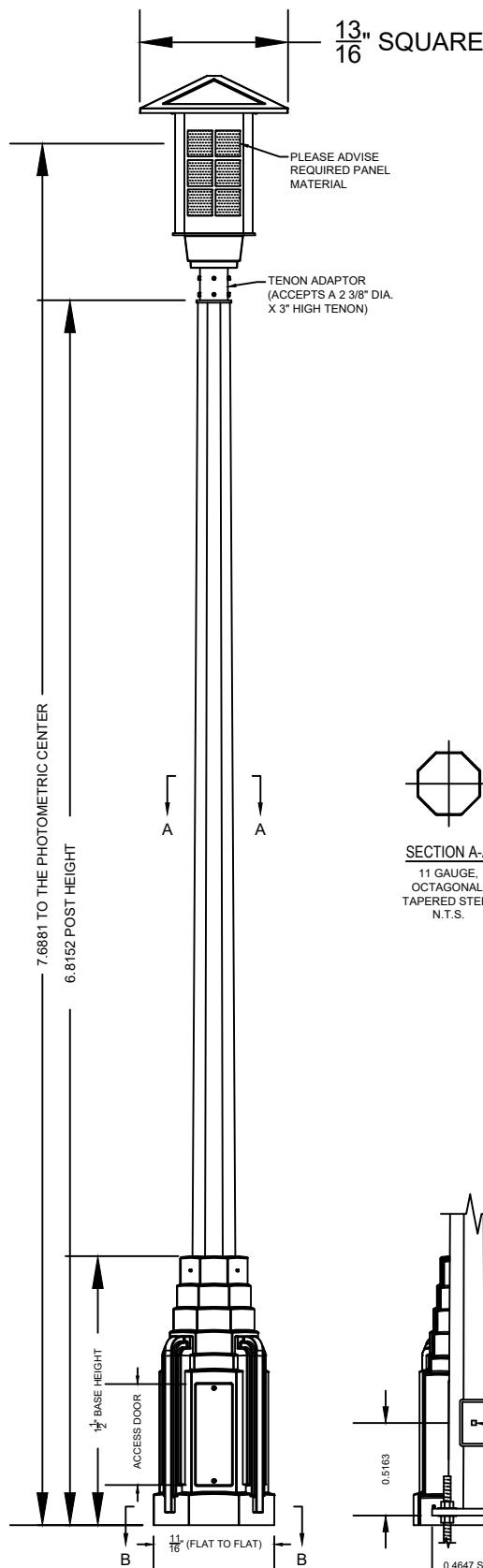
 NOTICE DATE
06/01/18

 ADOPTED DATE
06/01/18

 ENFORCEMENT DATE
07/01/18

 MPDIFIED DATE
06/01/18

 STANDARD DRAWING NO.
2300DM*



Spring City Electrical Mfg. Co.

HALL AND MAIN STREETS - P.O. BOX 19 - SPRING CITY, PA. 19475
PHONE (610) 948-4000 - FAX (610) 948-5577 - WWW.SPRINGCITY.COM

DESCRIPTION	THE 11'-0" FAIR PARK CAST ALUMINUM / STEEL POST WITH RANCH (SMALL) LED LUMINAIRE		
OPPORTUNITY	CITY OF MELISSA, TX DOWNTOWN LIGHTING		
SCALE N.T.S.	DRAWN BY: B.K.R.	DATE 04-28-2023	DRAWING NO. SPEC-34181

(OR APPROVED EQUAL)

LUMINAIRE SPECIFICATIONS:

STYLE: RANCH (SMALL)
HEIGHT: 24 1/4"
WIDTH: 16" SQUARE
MATERIAL: CAST ALUMINUM ALLOY AND FABRICATED ALUMINUM ALLOY
FINISH: POWDER COAT - PLEASE ADVISE FINISH COLOR
LAMPING: 50 WATT LED SYSTEM
VOLTAGE: ELECTRONICALLY WIRED AT 120-277 VOLTS
COLOR TEMP.: 3000K (WARM WHITE)
DISTRIBUTION: TYPE IV (ASYMMETRIC DISTRIBUTION)
PANELS: FROSTED LED GRADE ACRYLIC
SURGE: 10KV SURGE PROTECTION
MODIFIER: • ACCEPTS 2 3/8" DIA X 3" HIGH TENON (M1)
• 6-PANE CAGE PANELS (CP6)

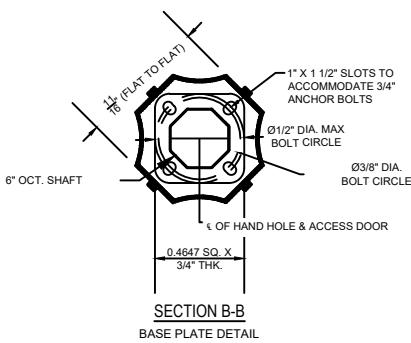
CATALOG NO.: ALMRHS-M1-LE050-EVX-1HC-30-CR4-YPLF-CP6-CU

LAMP POST SPECIFICATIONS:

STYLE: FAIR PARK STEEL
HEIGHT: 11'-0"
BASE: 13" (FLAT TO FLAT)
MATERIAL:

- SHAFT: 11 GAUGE - OCTAGONAL, TAPERED STEEL MONOTUBE CONSTRUCTION
- BASE: ONE PIECE (SLIP OVER) CAST ALUMINUM ALLOY A.N.S.I. 356 PER A.S.T.M. B26-95

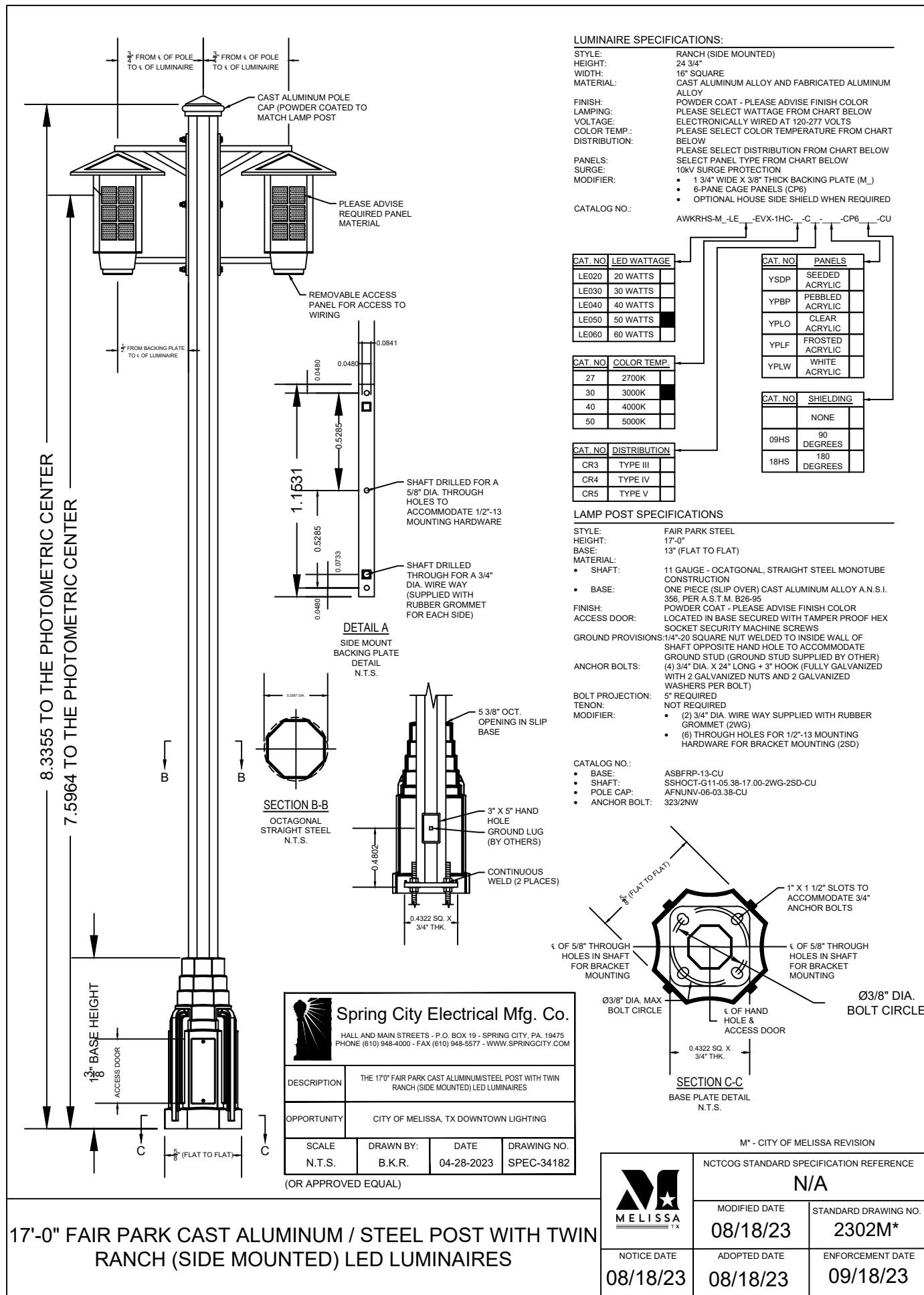
FINISH: POWDER COAT - PLEASE ADVISE FINISH COLOR
ACCESS DOOR: LOCATED IN BASE SECURED WITH TAMPER PROOF HEX SOCKET SECURITY MACHINE SCREWS
GROUND PROVISIONS: 1/4"-20 SQUARE NUT WELDED TO INSIDE WALL OF SHAFT OPPOSITE HAND HOLE TO ACCOMMODATE GROUND STUD (GROUND STUD SUPPLIED BY OTHER)
ANCHOR BOLTS: (4) 3/4" DIA. X 24" LONG + 3" HOOK (FULLY GALVANIZED WITH 2 GALVANIZED NUTS AND 2 GALVANIZED WASHERS PER BOLT)
BOLT PROJECTION: 5" REQUIRED
TENON: 2 3/8" DIA. X 3" HIGH (TO ACCEPT LUMINAIRE)
CATALOG NO.: ASBFRP-13-CU
• BASE: SSHOC-G11-06.00-11.00-TN2.38-3.00-CU
• SHAFT: 323/2NW
• ANCHOR BOLT:



M* - CITY OF MELISSA REVISION

 MELISSA	NCTCOG STANDARD SPECIFICATION REFERENCE	
	N/A	
MODIFIED DATE 08/18/23	STANDARD DRAWING NO. 2301M	
NOTICE DATE 08/18/23	ADOPTED DATE 08/18/23	ENFORCEMENT DATE 09/18/23

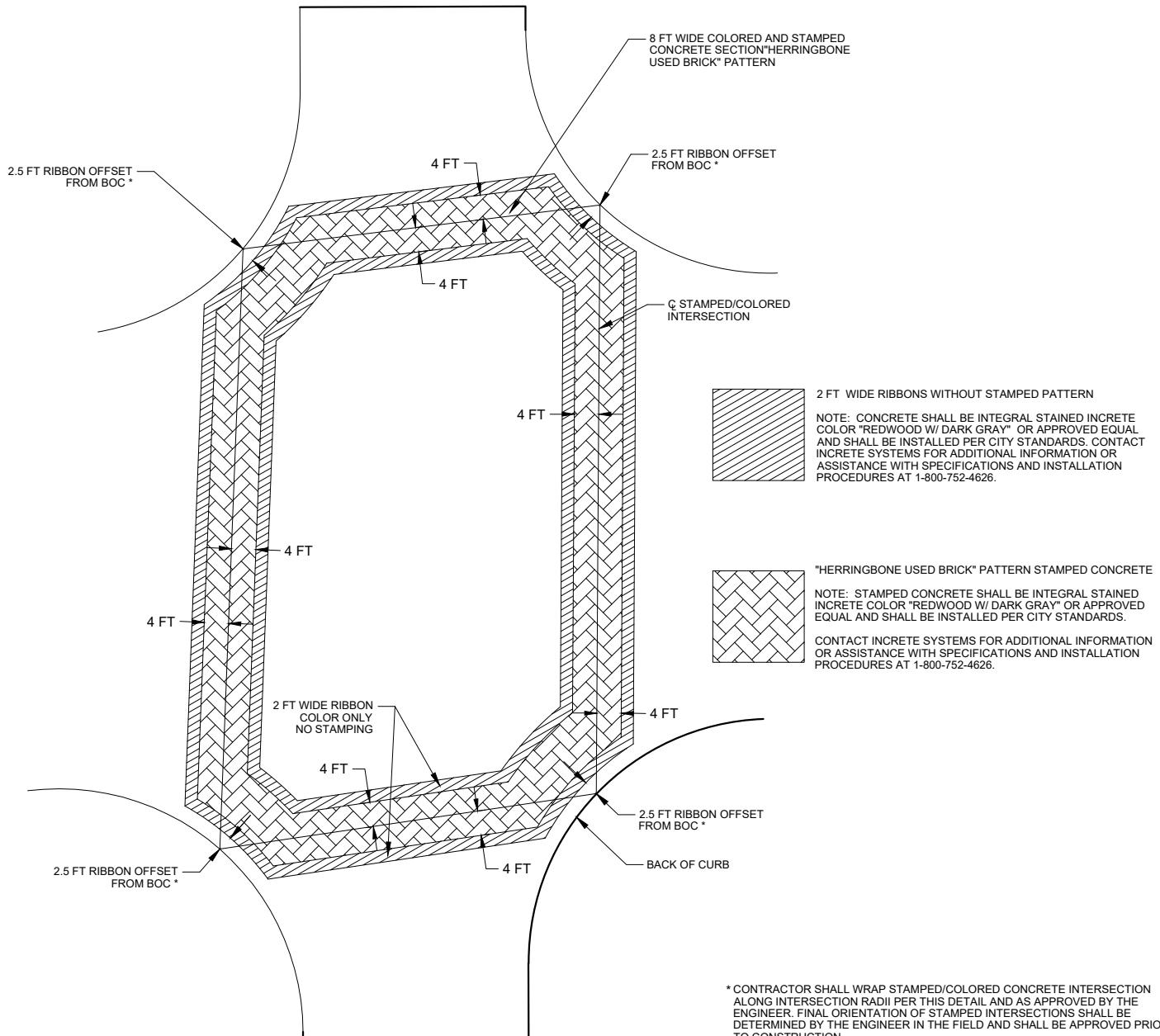
11'-0" FAIR PARK CAST ALUMINUM / STEEL POST
WITH RANCH (SMALL) LED LUMINAIRE



NOTES:
CONTRACTOR SHALL STAKE LOCATION OF ALL STAMPED/COLORED CONCRETE FOR APPROVAL FROM THE ENGINEER PRIOR TO CONSTRUCTION.

CONTRACTOR SHALL PROVIDE SAMPLES OF ALL INTEGRAL COLORED CONCRETE AND STAMP PATTERNS PRIOR TO ANY APPLICABLE CONSTRUCTION.

FOR STAMPED/COLORED INTERSECTIONS IN A "U" PATTERN (I.E. FOR A 3-LEG INTERSECTION), STAMPED/COLORED CONCRETE SHALL NOT ENCROACH THE PROPOSED BICYCLE LANE CROSSING THE MISSING LEG OF THE INTERSECTION AS SHOWN ON THE PLANS AND TYPICAL SECTIONS. NOTE THAT THIS ONLY APPLIES TO INTERSECTIONS WITH TRAFFIC CONTROL MEASURES (I.E. SIGNAL OR STOP CONDITION) ON THE MINOR STREET AND NO CONTROL ON THE MAJOR STREET.



STAMPED/COLORED CONCRETE INTERSECTION

CITY OF MELISSA

M* - CITY OF MELISSA REVISION

		NCTCOG STANDARD SPECIFICATION REFERENCE	
MODIFIED DATE	STANDARD DRAWING NO.	4/5/18	2310M*
NOTICE DATE	ADOPTED DATE	4/5/18	5/5/18